



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

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FISHER ARNOLD, INC.
9180 CRESTWYN HILLS DRIVE
MEMPHIS, TN 38125
JOHN M. REESE, P.E. 118371

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC DESIGN DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E, 2E1
TABULATED QUANTITIES	2F
UTILITY NOTES AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1

YEAR	PROJECT NO.	SHEET NO.
2026	STP/HSP-54(57)	ROADWAY-SIGN1

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**SIGNATURE
SHEET**

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2026	1
FED. AID PROJ. NO.	STP/HSIP-54(57)	
STATE PROJ. NO.	38S054-F3-005	
STATE PROJ. NO.	38S054-F8-005	
STATE PROJ. NO.	38S054-M3-005	

HAYWOOD COUNTY

STATE ROUTE 54
FROM NEAR THOMAS STREET
TO CROCKETT COUNTY LINE

RESURFACE & SAFETY

SCRUB SEAL, MICROSURFACE/411TL, PAVEMENT MARKINGS AND GUARDRAIL

STATE HIGHWAY NO. 54 F.A.H.S. NO. N/A



PROJECT LOCATION

BRIDGE ID. # 38SR0540003 , 38SR0540005 , 38SR0540007 , 38SR0540009 ,
38SR0540011 , 38SR0540013 , 38SR0540015 , 38SR0540017 , 38SR0540019 ,
38SR0540021 , 38SR0540023 , 38SR0540025 , 38SR0540027 , 38SR0540029

STP/HSIP-54(57)
38S054-F3-005
END PROJECT NO. 38S054-F8-005 RESURFACE & SAFETY
L.M. 24.63 TO CROCKETT COUNTY LINE

- BRIDGE REPAIR PROJECT NO. 385054-M3-005
- BRIDGE ID. #38SR0540025
- L.M. 22.86 BRANCH
- BRIDGE ID. #38SR0540023
- L.M. 22.69 OVERFLOW
- BRIDGE ID. #38SR0540021
- L.M. 22.49 OVERFLOW
- BRIDGE ID. #38SR0540019
- L.M. 22.36 SOUTH FORKED DEER RIVER
- BRIDGE ID. #36SR0540017
- L.M. 22.25 OVERFLOW
- BRIDGE ID. #38SR0540015
- L.M. 22.16 OVERFLOW
- BRIDGE ID. #38SR0540013
- L.M. 22.02 SANDY SLOUGH
- BRIDGE ID. #36SR0540007
- L.M. 16.84 NIXON CREEK

STP/HSIP-54(57)
38S054-F3-005
BEGIN PROJECT NO. 38S054-F8-005 RESURFACE & SAFETY
L.M. 13.62 FROM NEAR THOMAS STREET

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

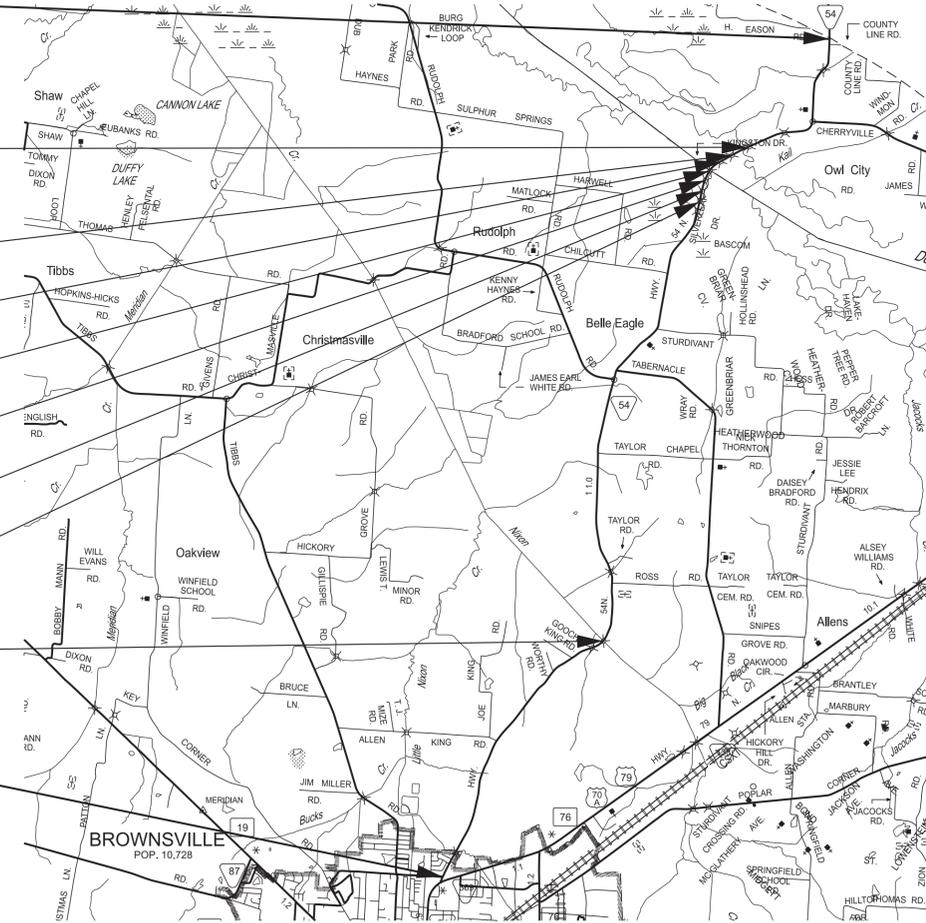
TDOT PROJECT MANAGER: LYNN EVANS, P.E., REGION 4

DESIGN FIRM : FISHER ARNOLD, INC.

DESIGNER : JAMES HUTCHESON DUNAVANT CHECKED BY JOHN REESE, P.E.

P.E. NO. 98043-4283-04

PIN NO. 135897.00



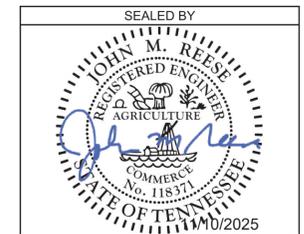
SCALE: 1"= 5280'



PROJECT LENGTH 11.01 MILES
TOTAL LANE MILES RESURFACED 21.55 MILES



NO EXCLUSIONS



APPROVED: *Shane Hester*
SHANE HESTER, CHIEF ENGINEER

DATE:

APPROVED: *Will Reid*
WILL REID, COMMISSIONER

TRAFFIC COUNTER & WEATHER STATIONS	
STATION LOCATION	LOG MILE
TC STATION 31	13.695
TC STATION 10	20.860

TRAFFIC DATA	
ADT (2026)	2532
POSTED SPEED L.M. 13.62 TO 14.03	45 MPH
POSTED SPEED L.M. 14.03 TO 24.63	55 MPH

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS, AND	
STANDARD TRAFFIC DESIGN DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E, 2E1
TABULATED QUANTITIES	2F
UTILITY NOTES AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1
BRIDGE REPAIR PLANS	B-1
NO UTILITY SHEETS ARE INCLUDED IN THIS SET OF PLANS.	

STANDARD ROADWAY DRAWINGS

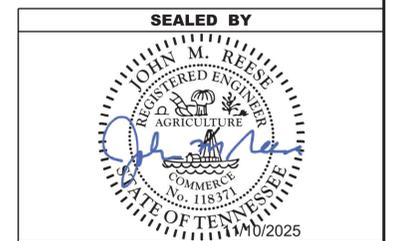
DWG.	REV.	DESCRIPTION
10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
10-107.00 SAFETY DESIGN AND GUARDRAILS		
S-GR31-1	10-31-25	GUARDRAIL DETAILS
S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2P	10-16-20	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL
S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)
S-GRA-4	03-01-23	IN-LINE GUARDRAIL ANCHOR TO PRIVATE DRIVE

STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
10-204.00 DESIGN - TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-40	03-26-25	RIGHT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-41	03-26-25	LEFT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSP-54(57)	1A

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**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

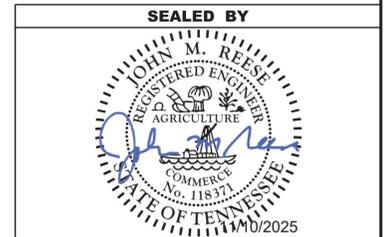
ROADWAY INDEX
STANDARD ROADWAY
DRAWINGS AND
STANDARD TRAFFIC
DESIGN DRAWINGS

11/7/2025 12:04:05 PM \\MEGNYTDRIVE\PROJECTS\HDR\INCOR\0009\TR\TRANSPORTATION\FEBRUARY 2026\135897.00 - SR54 - HAYWOOD CO\1B - PROJECT COMMITMENTS.DGN

PROJECT COMMITMENTS

COMMITMENT ID	SOURCE DIVISION	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON THE FOLLOWING BRIDGES AND NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. BRIDGE NO. 38SR0540007 SR-54 OVER NIXON CREEK LM 16.84 (38-SR054-16.84) BRIDGE NO. 38SR0540013 SR-54 OVER OVERFLOW LM 22.02 (38-SR054-22.02) BRIDGE NO. 38SR0540015 SR-54 OVER OVERFLOW LM 22.16 (38-SR054-22.16) BRIDGE NO. 38SR0540017 SR-54 OVER OVERFLOW LM 22.25 (38-SR054-22.25) BRIDGE NO. 38SR0540019 SR-54 OVER SOUTH FORK DEER RIVER LM 22.36 (38-SR054-22.36) BRIDGE NO. 38SR0540021 SR-54 OVER OVERFLOW LM 22.49 (38-SR054-22.49) BRIDGE NO. 38SR0540023 SR-54 OVER OVERFLOW LM 22.69 (38-SR054-22.69) BRIDGE NO. 38SR0540025 SR-54 OVER OVERFLOW LM 22.86 (38-SR054-22.86)	BRIDGES
EDHZ002	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSIP-54(57)	1B



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

PROJECT
COMMITMENTS

ESTIMATED ROADWAY QUANTITIES

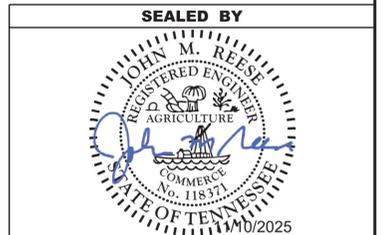
ITEM NO.	DESCRIPTION	UNIT	QUANTITY		TOTAL QUANTITY
			38S054-F8-005	38S054-F3-005	
(1)	202-03.01 REMOVAL OF ASPHALT PAVEMENT	S.Y.	228		228
	203-06 WATER	M.G.	23		23
(2)	303-02 MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	3006		3006
(1)	307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	105		105
(3)(4)(5)	414-04.03 ASPHALT EMULSION (SCRUB SEAL)	TON	213		213
(3)(4)(6)	414-04.04 MINERAL AGGREGATE (SCRUB SEAL)	TON	1503		1503
(7)	705-04.09 EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH		5	5
(7)	705-06.11 GR TERMINAL (IN-INLINE) MASH TL-3	EACH		1	1
(7)	705-06.20 TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH		5	5
(7)	706-01 GUARDRAIL REMOVED	L.F.		300	300
(7)	706-06.03 RADIUS RAIL	L.F.		14	14
(7)	706-10.26 ROUNDED END ELEMENT	EACH		1	1
(8)	712-01 TRAFFIC CONTROL	LS	1		1
(9)	712-06 SIGNS (CONSTRUCTION)	S.F.	1720		1720
(10)(11)	716-01.21 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	450	305	755
	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	450		450
(12)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	83.2		83.2
	716-08.20 REMOVAL OF PAVEMENT MARKING (LINE)	L.M.	41.6		41.6
(13)(14)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	41.6		41.6
	717-01 MOBILIZATION	LS	1		1
	ALTERNATE AA1				
(3)(15)(16)(17)	403-01.01 BITUMINOUS MATERIAL FOR TACK COAT (MICRO-SURFACING)	TON	20		20
(3)(15)(18)	414-03.01 EMULSIFIED ASPHALT FOR MICRO-SURFACING	TON	270		270
(3)(15)(19)	414-03.02 AGGREGATE FOR MICRO SURFACING	TON	2237		2237
(20)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	219		219
	ALTERNATE AA2				
(3)(17)(21)	403-02.01 TRACKLESS TACK COAT	TON	32		32
(3)(21)(22)	411-03.07 ACS MIX(PG64-22) THIN LIFT ASPHALT	TON	4814		4814
(23)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	357		357

NOTE: THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT.

FOOTNOTES

- (1) TO BE USED FOR A MIX BREAKOUTS ONLY.
- (2) INCLUDES 155 FOR A MIX BREAKOUTS.
- (3) THE TREATMENT OF SIDE ROADS, BUSINESS ENTRANCES, FIELD ENTRANCES AND DRIVEWAYS WILL NOT BE INCLUDED IN THIS PROJECT
- (4) CONTRACTOR SHALL KEEP THE SCRUB SEAL INSIDE OF SHOULDER SCORING.
- (5) INCLUDES 2 TONS FOR EXTRA WIDTH PAVING FOR WIDE SHOULDERS.
- (6) INCLUDES 12 TONS FOR EXTRA WIDTH PAVING FOR WIDE SHOULDERS.
- (7) SEE PROPOSED GUARDRAIL (RESURFACING) TABULATION ON SHEET 2F.
- (8) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION). SEE TABULATED QUANTITIES ON SHEET 2F.
- (10) INCLUDES 25 SRPM'S FOR EDGE LINES FROM L.M. 13.62 TO L.M. 13.93.
- (11) SNOWPLOWABLE MARKERS SHALL NOT BE CUT INTO MICROSURFACE UNTIL AT LEAST TWO WEEKS OF DRYING.
- (12) ITEM TO BE USED FOR TEMPORARY PAVEMENT MARKING ONLY.
- (13) ITEM TO BE USED FOR FINAL PAVEMENT MARKING ONLY.
- (14) THERMOPLASTIC SHALL NOT BE APPLIED TO MICROSURFACE UNTIL AT LEAST TWO WEEKS OF DRYING.
- (15) CONTRACTOR SHALL KEEP THE MICROSURFACING INSIDE OF SHOULDER SCORING.
- (16) NO PAYMENT WILL BE MADE DIRECTLY FOR THE WATER REQUIRED TO DILUTE THE TACK COAT.
- (17) INCLUDES 1 TON FOR EXTRA WIDTH PAVING FOR WIDE SHOULDERS.
- (18) INCLUDES 3 TONS FOR EXTRA WIDTH PAVING FOR WIDE SHOULDERS.
- (19) INCLUDES 18 TONS FOR EXTRA WIDTH PAVING FOR WIDE SHOULDERS.
- (20) TO BE USED AT A DEPTH OF 0.63" +/- FOR 225' TAPERS AT PROJECT ENDS AND PORTLAND CEMENT CONCRETE BRIDGES.
- (21) CONTRACTOR SHALL KEEP THE 411TL INSIDE OF SHOULDER SCORING.
- (22) INCLUDES 37 TONS FOR EXTRA WIDTH PAVING FOR WIDE SHOULDERS.
- (23) TO BE USED AT A DEPTH OF 1.03" +/- FOR 225' TAPERS AT PROJECT ENDS AND PORTLAND CEMENT CONCRETE BRIDGES.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HISP-54(57)	2

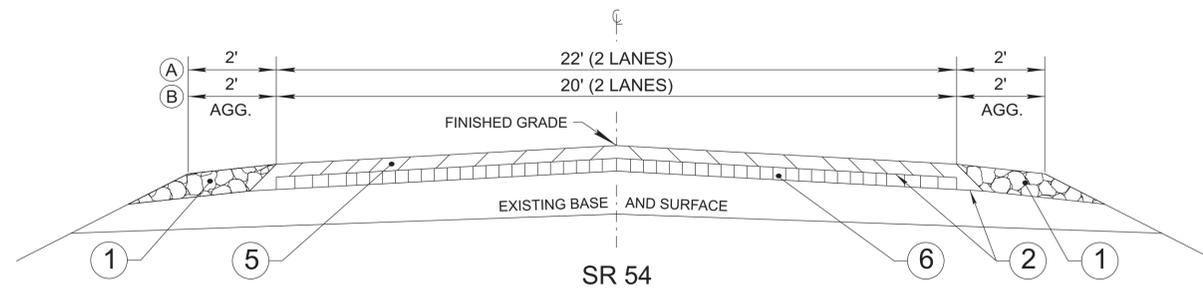


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

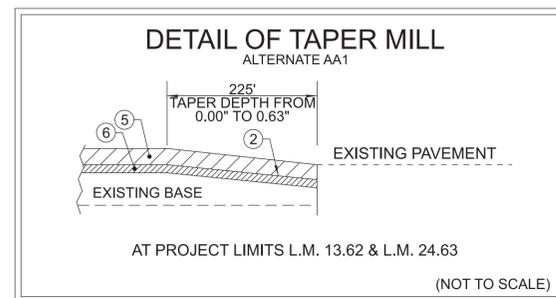
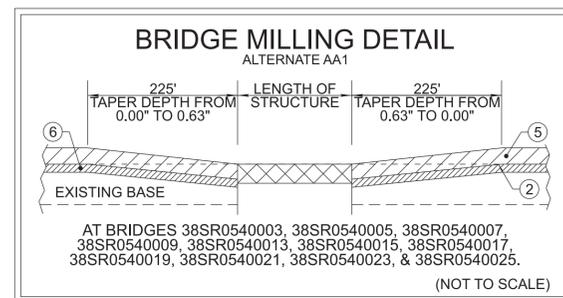
ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSP-54(57)	2B

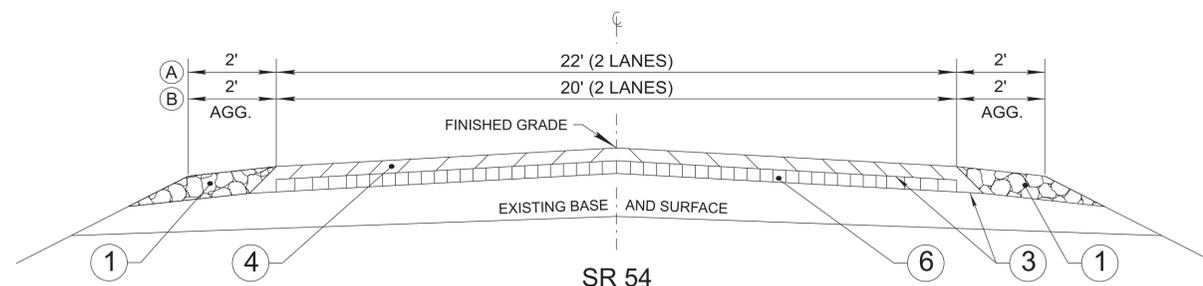
ALTERNATE AA1 MICRO-SURFACING



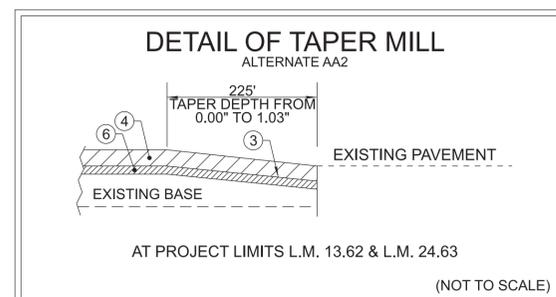
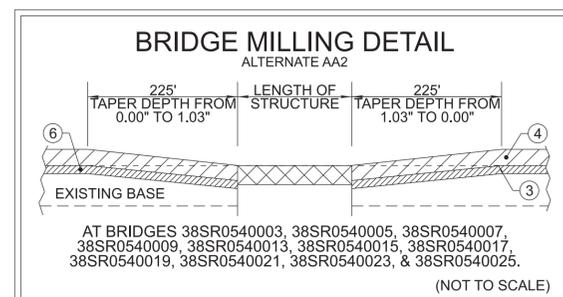
(A) FROM: L.M. 13.62 TO L.M. 24.32
(B) FROM: L.M. 24.32 TO L.M. 24.63



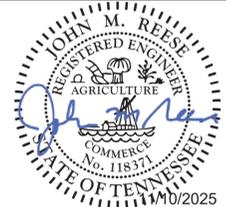
ALTERNATE AA2 THIN LIFT D



(A) FROM: L.M. 13.62 TO L.M. 24.32
(B) FROM: L.M. 24.32 TO L.M. 24.63



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

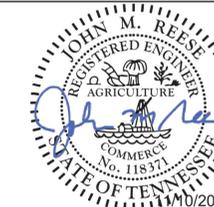
TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSIP-54(57)	2B1

PROPOSED PAVEMENT SCHEDULE

<p>① MINERAL AGGREGATE @ 2.00"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"</p>	<p>⑤ MICRO-SURFACING ITEM 414-03.01 EMULSIFIED ASPHALT FOR MICRO-SURFACING EMULSIFIED ASPHALT (AT 12% OF THE DRY WEIGHT OF AGGREGATE) ITEM 414-03.02 AGGREGATE FOR MICRO-SURFACING AGGREGATE (AT 24 LBS./S.Y.)</p>
<p>② TACK COAT (TC) FOR MICRO-SURFACING ITEM 403-01.01 BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD</p>	<p>⑥ SCRUB SEAL ITEM 414-04.03 ASPHALT EMULSION (SCRUB SEAL) ASPHALT EMULSION (AT 0.25 - 0.35 GAL./S.Y.) ITEM 414-04.04 MINERAL AGGREGATE (SCRUB SEAL) MINERAL AGGREGATE (AT 18 - 25 LBS./S.Y.) (SCRUB SEAL SHALL NOT EXTEND PAST THE INSIDE EDGE OF THE RUMBLE STRIPES.)</p>
<p>③ TRACKLESS TACK COAT ITEM 403-02.01 TRACKLESS TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD</p>	<p>COLD PLANING ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT (THIS ITEM TO BE USED AT PROJECT ENDS AND BRIDGES.)</p>
<p>④ ASPHALTIC CONCRETE SURFACE (HM) @ 0.65"± THICK (APPROX. 65 LBS./S.Y.) ITEM 411-03.07 ACS MIX (PG64-22) THIN LIFT ASPHALT</p>	

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**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE ENGINEER.
 - b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE ENGINEER.
 - c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
 - d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (1) THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

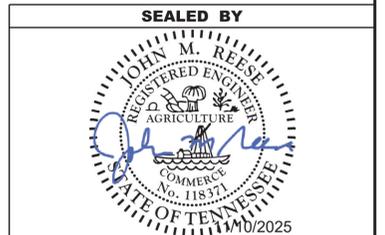
- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSP-54(57)	2C



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

GENERAL
NOTES

SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSIP-54(57)	2D

RESURFACING

- (1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (6) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.
- (8) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

SIGNS

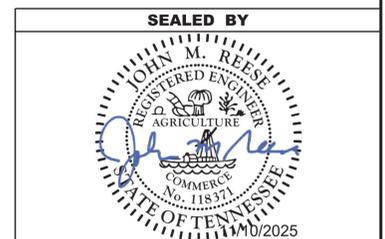
- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

- (1) ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.
- (4) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE PROJECT MANAGER SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL

- (20) EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRECONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES COLD-PLANING, SCRUB SEAL, 411TL, MICRO-SURFACING, GUARDRAIL, SNOWPLOWABLE RAISED PAVEMENT MARKERS, AND PAVEMENT MARKINGS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

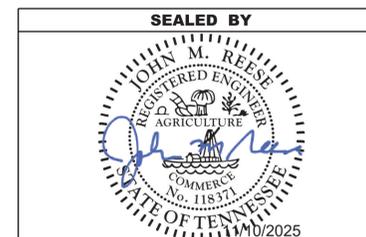
- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.

- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HISP-54(57)	2E



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

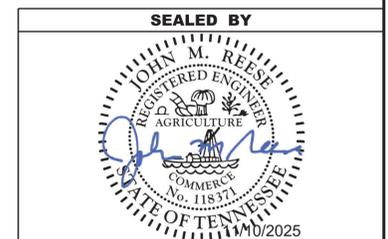
ENVIRONMENTAL NOTES

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSIP-54(57)	2E1



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

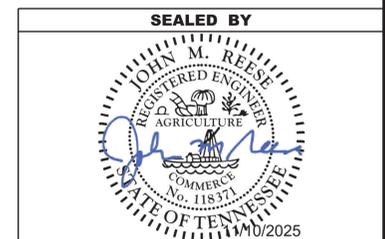
ENVIRONMENTAL
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSP-54(57)	2F

BRIDGE NOTES			
BRIDGE NUMBER	LOCATION LOG MILE	BRIDGE LENGTH	BRIDGE DECK NOTES
38SR0540003	13.91	36'	LEAVE AS IS
38SR0540005	16.73	36'	LEAVE AS IS
38SR0540007	16.84	94'	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY
38SR0540009	17.63	34'	LEAVE AS IS
38SR0540011	21.95	47'	PAVE WITH PLANS MIX / TREATMENT TYPE
38SR0540013	22.02	75'	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY REPLACE BRIDGE JOINTS
38SR0540015	22.16	206'3"	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY REPLACE BRIDGE JOINTS
38SR0540017	22.25	151'2"	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY REPLACE BRIDGE JOINTS
38SR0540019	22.36	175' 1 1/4"	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY REPLACE BRIDGE JOINTS
38SR0540021	22.49	151' 3"	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY REPLACE BRIDGE JOINTS
38SR0540023	22.69	181' 3"	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY REPLACE BRIDGE JOINTS
38SR0540025	22.86	121' 3"	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY REPLACE BRIDGE JOINTS
38SR0540027	22.25	47'	PAVE WITH PLANS MIX / TREATMENT TYPE
38SR0540029	24.26	35'	PAVE WITH PLANS MIX / TREATMENT TYPE

PROPOSED GUARDRAIL (RESURFACING)								
SIDE	LOG MILE	EARTH PAD	GUARDRAIL				TERMINAL ANCHORS	
		TYPE 38 GR END TREATMENT 705-04.09 EACH	RADIUS RAIL 706-06.03 (L.F.)	ROUND END ELEMENT 706-10.26 (EACH)	GUARDRAIL GR TRANSITION 27 IN TO 31 IN 705-02.10 EACH	GUARDRAIL REMOVED 706-01 (L.F.)	IN-LINE MASH TL3 705-06.11 (EACH)	TYPE 38 MASH TL3 (46.875') 705-06.20 (EACH)
LT	RT							
X		13.908	1					
	X	13.940	1			50		1
	X	16.896	1		1	50		1
	X	21.360	1			50		1
X		21.916	1		1	50		1
X		23.235			1	50		1
X		24.121				50		
	X	24.258		14	1		1	
		TOTALS	5	14	1	3	300	5

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES		S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	x W			
G20-1	ROAD WORK NEXT 12 MILES	48"	x 24"	8	2	16
G20-2	END ROAD WORK	48"	x 24"	8	16	128
W8-11	UNEVEN LANES	48"	x 48"	16	60	960
W8-15	GROOVED PAVEMENT	48"	x 48"	16	8	128
W8-15P	MOTORCYCLE PLAQUE	30"	x 24"	5	8	40
W20-1	ROAD WORK 1 MILE	48"	x 48"	16	2	32
W20-1	ROAD WORK 1/2 MILE	48"	x 48"	16	2	32
W20-1	ROAD WORK 1000 FT	48"	x 48"	16	2	32
W20-1	ROAD WORK AHEAD	48"	x 48"	16	16	256
W20-4	ONE LANE ROAD 1000 FT - PORTABLE	48"	x 48"	16	2	32
W20-7A	FLAGGER SYMBOL - PORTABLE	48"	x 48"	16	2	32
W21-2	FRESH OIL - PORTABLE	48"	x 48"	16	2	32
THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.					TOTAL	1720



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSP-54(57)	3

UTILITY NOTES

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

COMMUNICATIONS:

AT&T

315 E. COLLEGE STREET
 JACKSON, TN 38301
 CONTACT: DANIEL R. POTTS
 OFFICE PHONE: 901 488 2359
 CELL PHONE:
 Email: DP7607@ATT.COM

ELECTRIC:

STEMC

1009 E. MAIN STREET
 BROWNSVILLE, TN 38012
 CONTACT: JOSHUA KENNEDY
 OFFICE PHONE: 731 585 0531
 CELL PHONE:
 Email: PROJECTS@STEMC.COM

ELECTRIC / GAS / WATER / WASTEWATER:

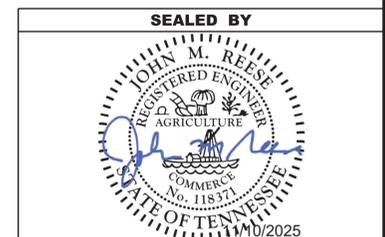
BROWNSVILLE ENERGY AUTHORITY

P.O. BOX 424
 BROWNSVILLE, TN 38012
 CONTACT: RUSS STOOTS
 OFFICE PHONE: 731 696 2908
 CELL PHONE:
 Email: RSTOOTS@BUDUTIL.COM

WATER:

COUNTY WIDE UTILITY DISTRICT

70 SOUTH CAVALIER DRIVE
 ALAMO, TN 38001
 CONTACT: MICHEAL JACKSON
 OFFICE PHONE: 731 696 2908
 CELL PHONE:
 Email: MICHAEL@COUNTYUTILITY.COM



**STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION**

UTILITY NOTES
 AND
 UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	STP/HSP-54(57)	T1

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
 - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.
 - a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

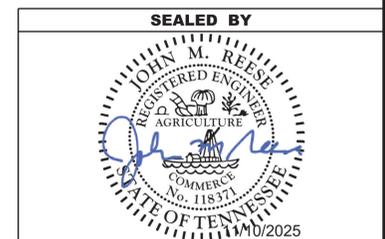
THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

BRIAN KENNETH EGLI
2025.11.04 14:52:24 -06'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME **SHEET NO.**

SIGNATURE SHEET	-----	STRUCTURE-SIGN 1
INDEX OF DRAWINGS	-----	B-1
BRIDGE TABULATION, ESTIMATED QUANTITIES	-----	B-2
TYPE I THIN EPOXY OVERLAY NOTES	-----	B-3
APPROACH SLAB REPAIR DETAIL NOTES	-----	B-4
BRIDGE DECK REPAIR NOTES	-----	B-5
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540007)	-----	B-6
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540013)	-----	B-7
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540015)	-----	B-8
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540017)	-----	B-9
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540019)	-----	B-10
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540021)	-----	B-11
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540023)	-----	B-12
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540025)	-----	B-13

YEAR	PROJECT NO.	SHEET NO.
2025	38S054-M3-005	STRUCTURE-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-1	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
-	-		
-	-		
-	-		
-	-		
-	-		

INDEX OF DRAWINGS

DWG. NO.

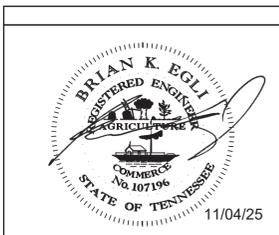
LAST REV. DATE

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INDEX OF DRAWINGS	-----	B-1
BRIDGE TABULATION, ESTIMATED QUANTITIES	-----	B-2
TYPE I THIN EPOXY OVERLAY NOTES	-----	B-3
APPROACH SLAB REPAIR DETAIL NOTES	-----	B-4
BRIDGE DECK REPAIR NOTES	-----	B-5
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540007)	-----	B-6
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540013)	-----	B-7
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540015)	-----	B-8
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540017)	-----	B-9
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540019)	-----	B-10
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540021)	-----	B-11
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540023)	-----	B-12
PLAN VIEW AND PHASE CONSTRUCTION (38SR0540025)	-----	B-13

INDEX OF REFERENCE DRAWINGS

DWG. NO.

LAYOUT OF BRIDGE	-----	M-270-68
SUPERSTRUCTURE	-----	M-221-71
LAYOUT OF BRIDGE	-----	M-131-1
SUPERSTRUCTURE	-----	M-131-4
LAYOUT OF BRIDGE	-----	M-131-10
SUPERSTRUCTURE	-----	M-131-13
LAYOUT OF BRIDGE	-----	M-131-19
SUPERSTRUCTURE	-----	M-131-22
LAYOUT OF BRIDGE	-----	M-131-28
SUPERSTRUCTURE	-----	M-131-31
LAYOUT OF BRIDGE	-----	M-131-38
SUPERSTRUCTURE	-----	M-131-41
LAYOUT OF BRIDGE	-----	M-131-47
SUPERSTRUCTURE	-----	M-131-50
LAYOUT OF BRIDGE	-----	M-131-56
SUPERSTRUCTURE	-----	M-131-59
PAVEMENT AT BRIDGE ENDS	-----	K-86-144
PAVEMENT AT BRIDGE ENDS	-----	STD-1-3



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
INDEX OF DRAWINGS
38-SR54-16.84
OVER NIXON CREEK,
38-SR54-22.02
OVER SANDY SLOUGH,
38-SR54-22.16
OVER OVERFLOW,
38-SR54-22.25
OVER OVERFLOW,
38-SR54-22.36
OVER SOUTH FORKED
DEER RIVER,
38-SR54-22.49
OVER OVERFLOW,
38-SR54-22.69
OVER OVERFLOW,
38-SR54-22.86
OVER BRANCH
BR. NOS. 38SR0540007
38SR0540013
38SR0540015
38SR0540017
38SR0540019
38SR0540021
38SR0540023
38SR0540025
HAYWOOD COUNTY
2026

PIN NO.: 135897.00
DESIGN BY: _____ DATE: _____
DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
CHECKED BY: _____ DATE: _____

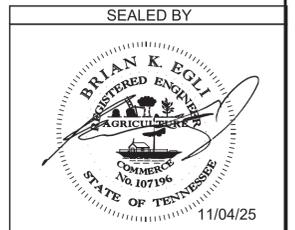
TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES

LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) S.Y.	604-10.53 CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	617-04.01 TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
38-SR54-16.84 OVER NIXON CREEK (38SR0540007)	M-270-68 M-270-71 STD-1-3	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)			517
38-SR54-22.02 OVER SANDY SLOUGH (38SR0540013)	M-131-1 M-131-4 K-86-144	BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	14	45	448
38-SR54-22.16 OVER OVERFLOW (38SR0540015)	M-131-10 M-131-13 K-86-144	BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	400		922
38-SR54-22.25 OVER OVERFLOW (38SR0540017)	M-131-19 M-131-22 K-86-144	BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	5		731
38-SR54-22.25 OVER SOUTH FORKED DEER RIVER (38SR0540019)	M-131-28 M-131-31 K-86-144	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)			810
38-SR54-22.49 OVER OVERFLOW (38SR0540021)	M-131-28 M-131-31 K-86-144	BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	43		731
38-SR54-22.69 OVER OVERFLOW (38SR0540023)	M-131-38 M-131-41 K-86-144	BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	11	15	731
38-SR54-22.86 OVER BRANCH (38SR0540025)	M-131-56 M-131-59 K-86-144	BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	22	29	832
TOTAL			495	89	5,722

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-2	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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BRIDGE REPAIR TRAFFIC CONTROL QUANTITIES			
Item #	Description	Unit	Quantity
712-04.01	Flexible Drums (Channelizing)	Each	170
712-06	Signs (Construction)	S.F.	64

BRIDGE REPAIR SIGN TABULATION							
M.U.T.C.D.	LEGEND \ DESCRIPTION	SIZE IN INCHES			S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
NO.		L	x	W			
W3-4	BE PREPARED TO STOP	48"	x	48"	16	2	32
W5-3	ONE LANE BRIDGE AHEAD	48"	x	48"	16	2	32
						TOTAL	64



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE TABULATION,
ESTIMATED QUANTITIES
38-SR54-16.84
OVER NIXON CREEK,
38-SR54-22.02
OVER SANDY SLOUGH,
38-SR54-22.16
OVER OVERFLOW,
38-SR54-22.25
OVER OVERFLOW,
38-SR54-22.36
OVER SOUTH FORKED
DEER RIVER,
38-SR54-22.49
OVER OVERFLOW,
38-SR54-22.69
OVER OVERFLOW,
38-SR54-22.86
OVER BRANCH
BR. NOS. 38SR0540007
38SR0540013
38SR0540015
38SR0540017
38SR0540019
38SR0540021
38SR0540023
38SR0540025
HAYWOOD COUNTY
2026

PIN NO.: 135897.00
DESIGN BY: KEVIN MARTINKO DATE: 9/29/25
DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
CHECKED BY: DATE:

TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

- A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.
- B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.
- C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.
- D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.
- E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.
- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

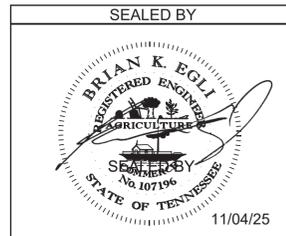
THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-3	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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**** SPECIAL NOTE:**
 THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACKCOAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY.
 MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACKCOAT OR DEBRIS REMOVAL.
 REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
**TYPE I THIN EPOXY
 OVERLAY NOTES**
 38-SR54-16.84
 OVER NIXON CREEK,
 38-SR54-22.02
 OVER SANDY SLOUGH,
 38-SR54-22.16
 OVER OVERFLOW,
 38-SR54-22.25
 OVER OVERFLOW,
 38-SR54-22.36
 OVER SOUTH FORKED
 DEER RIVER,
 38-SR54-22.49
 OVER OVERFLOW,
 38-SR54-22.69
 OVER OVERFLOW,
 38-SR54-22.86
 OVER BRANCH
 BR. NOS. 38SR0540007
 38SR0540013
 38SR0540015
 38SR0540017
 38SR0540019
 38SR0540021
 38SR0540023
 38SR0540025
 HAYWOOD COUNTY
 2026

PIN NO.: 135897.00
 DESIGN BY: _____ DATE: _____
 DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
 SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
 CHECKED BY: _____ DATE: _____

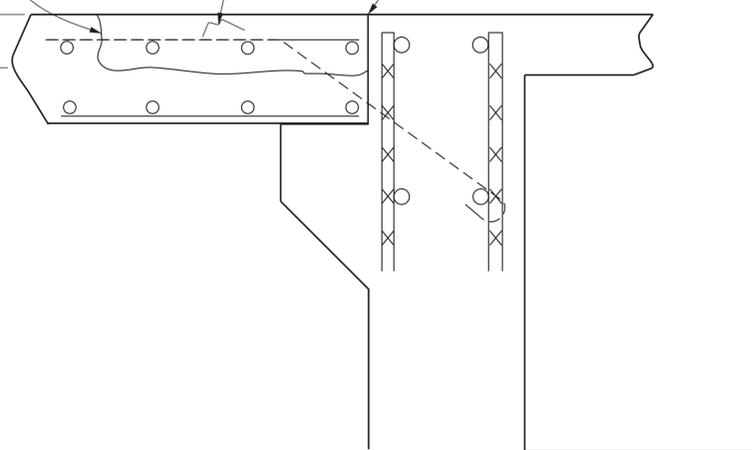
PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-4	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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USE AN EXTENDED RAPID SET CEMENTITIOUS PATCHING MATERIALS (FOR PATCHING MATERIAL REFER TDOT QUALIFIED PRODUCT LIST NO. 13.004)

INSTALL BOND BREAKER BEFORE PLACING CONCRETE FOR APPROACH SLAB REPAIR.

EDGES TO BE SAW CUT TO A DEPTH OF 1"

4" (MIN.)



CONCRETE APPROACH PAVEMENT REPAIR DETAILS (STRUCTURAL REPAIR)

NOTE: REMOVE CONCRETE TO A DEPTH OF 3/4" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR (S.Y.).

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT.) THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

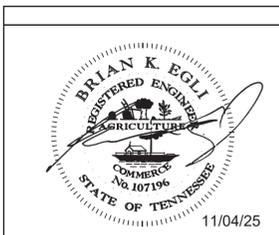
(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

(2) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

BRIDGE DECK REPAIR MATERIAL:

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS. MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.

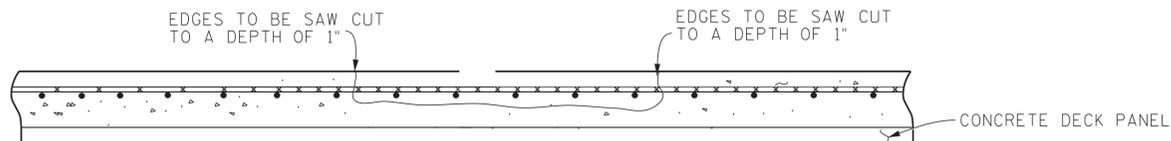


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
APPROACH SLAB REPAIR
DETAIL NOTES
38-SR54-22.02
OVER SANDY SLOUGH,
38-SR54-22.69
OVER OVERFLOW,
38-SR54-22.86
OVER BRANCH
BR. NOS. 38SR0540013
38SR0540023
38SR0540025
HAYWOOD COUNTY
2026

PIN NO.: 135897.00
DESIGN BY: _____ DATE: _____
DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
CHECKED BY: _____ DATE: _____

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-5	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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USE AN EXTENDED RAPID SET CEMENTITIOUS PATCHING MATERIALS (FOR PATCHING MATERIAL REFER TOOT QUALIFIED PRODUCT LIST NO.13.004)



SKETCH SHOWING DECK REPAIR (STRUCTURAL REPAIR):

NOTE: PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE BOTTOM BAR OF THE TOP MAT OF REINFORCING STEEL.

NOTE: PARTIAL DEPTH DECK REPAIRS: ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBARS AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

NOTE: ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

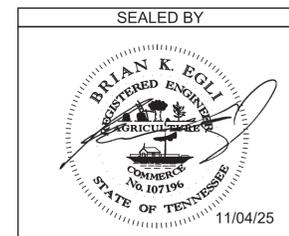
POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

- (1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 lb. CLASS SHALL NOT BE USED. TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.
- (2) CHIPPING HAMMERS OF THE 15 lb. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

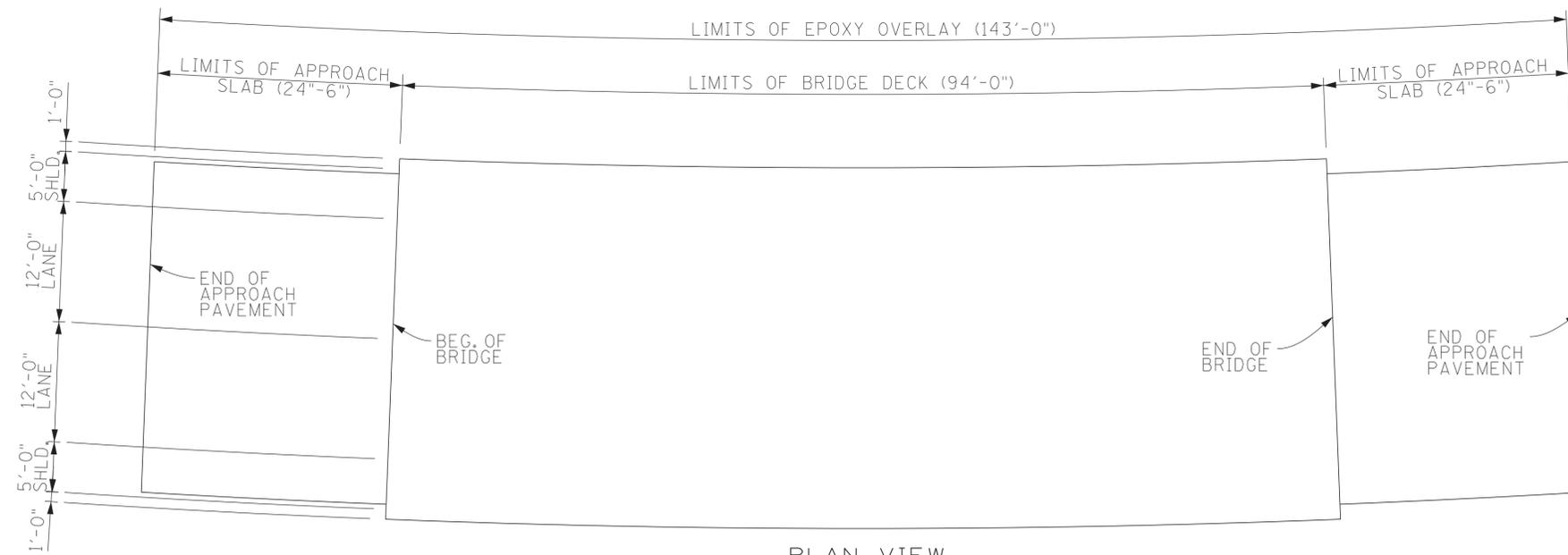
BRIDGE DECK REPAIR MATERIAL:

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS. MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS. MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.

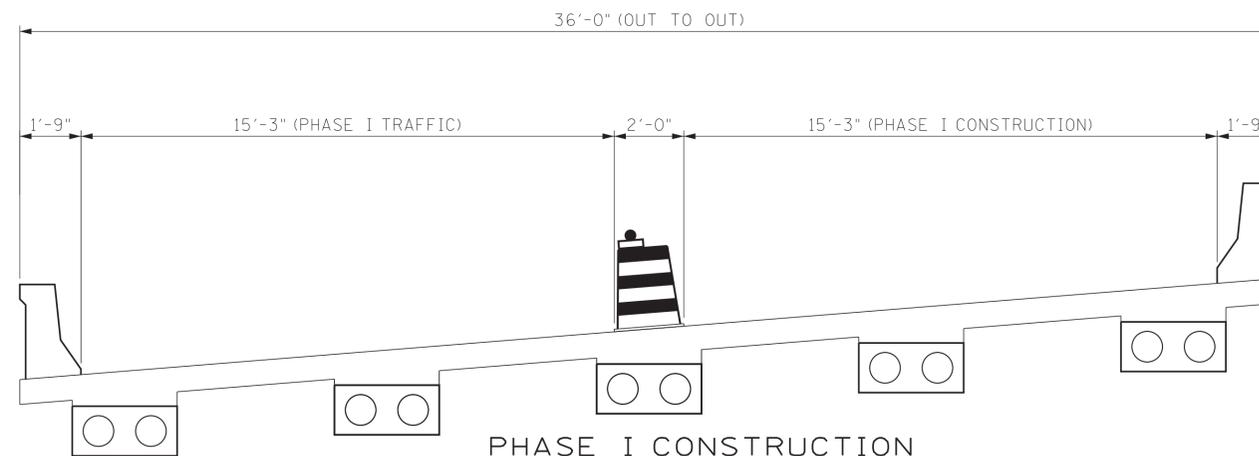


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE DECK
REPAIR NOTES
38-SR54-22.02
OVER SANDY SLOUGH,
38-SR54-22.16
OVER OVERFLOW,
38-SR54-22.25
OVER OVERFLOW,
38-SR54-22.49
OVER OVERFLOW,
38-SR54-22.69
OVER OVERFLOW,
38-SR54-22.86
OVER OVERFLOW
BR. NOS. 38SR0540013
38SR0540015
38SR0540017
38SR0540021
38SR0540023
38SR0540025
HAYWOOD COUNTY
2026

PIN NO.: 135897.00
DESIGN BY: _____ DATE: _____
DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
CHECKED BY: _____ DATE: _____

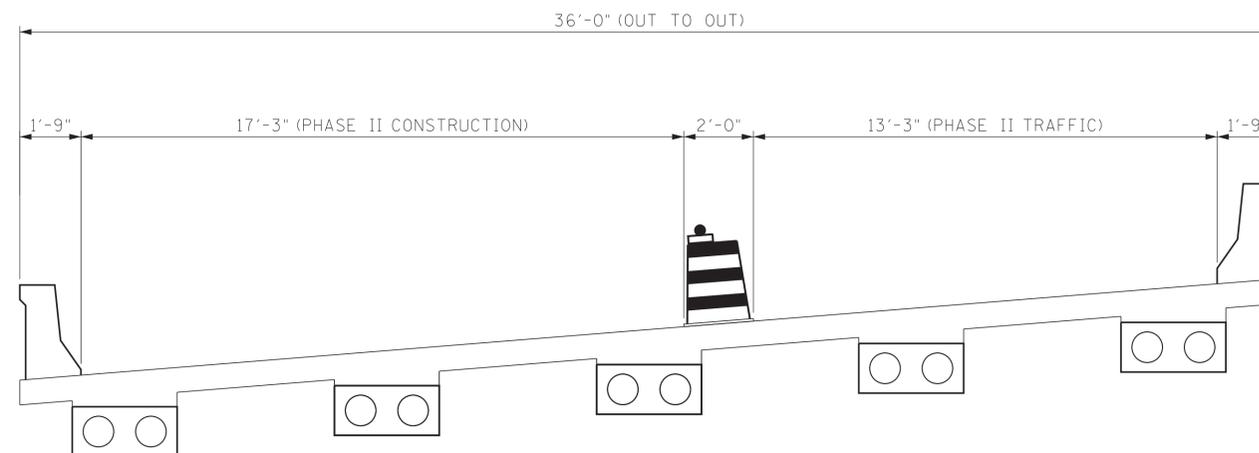


PLAN VIEW
BRIDGE ID. NO. 36S0540007



PHASE I CONSTRUCTION

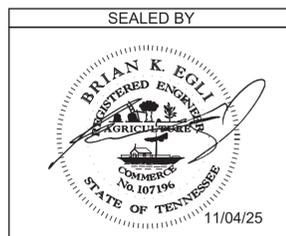
BRIDGE ID. NO. 36S0540007
(LOOKING AHEAD ON SURVEY)



PHASE II CONSTRUCTION

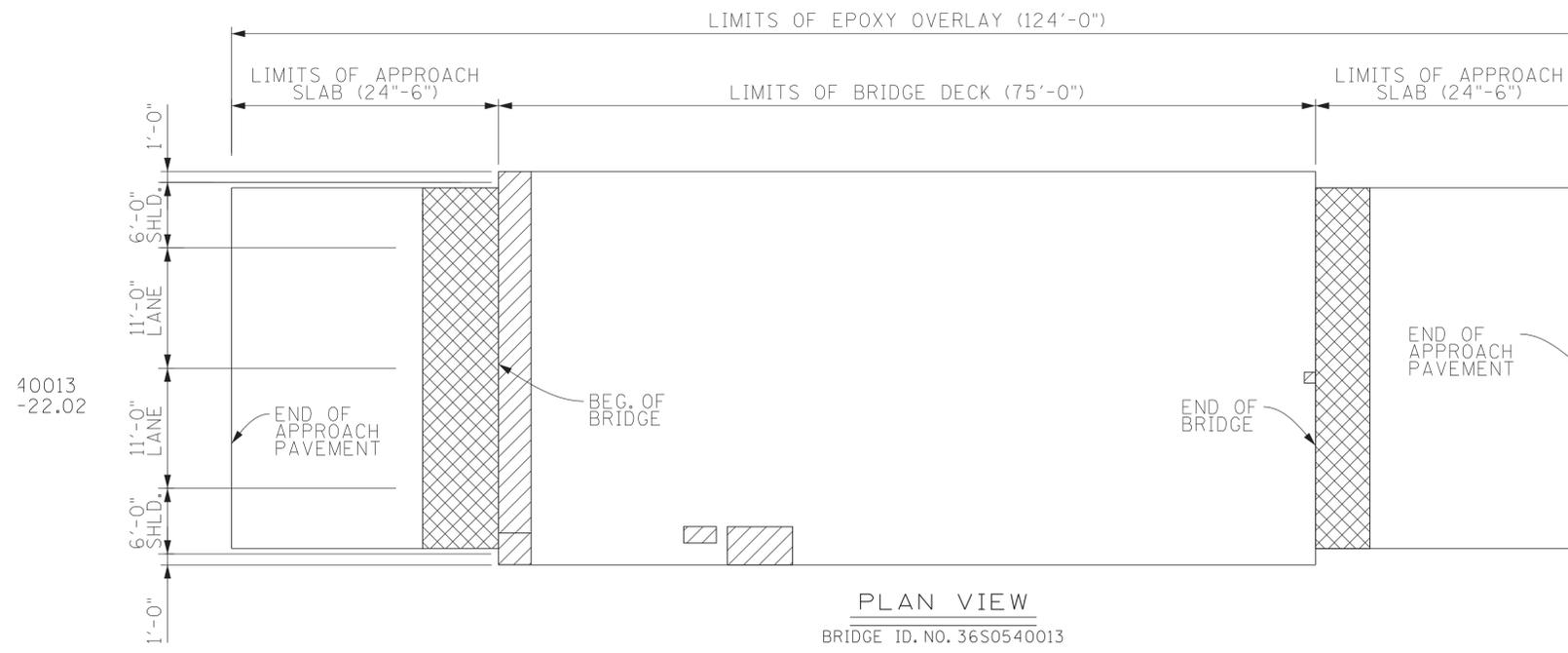
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(LOOKING AHEAD ON SURVEY)

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-6	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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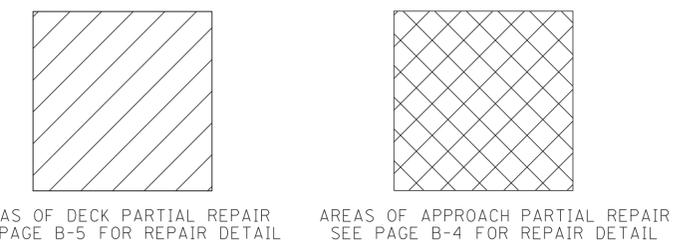


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW AND
PHASE CONSTRUCTION
38-SR54-16.84
OVER NIXON CREEK,
BR. NO. 38SR0540007
HAYWOOD COUNTY
2026

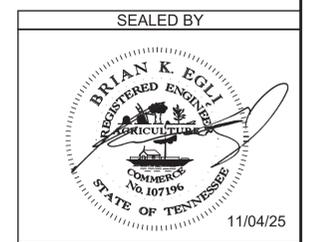
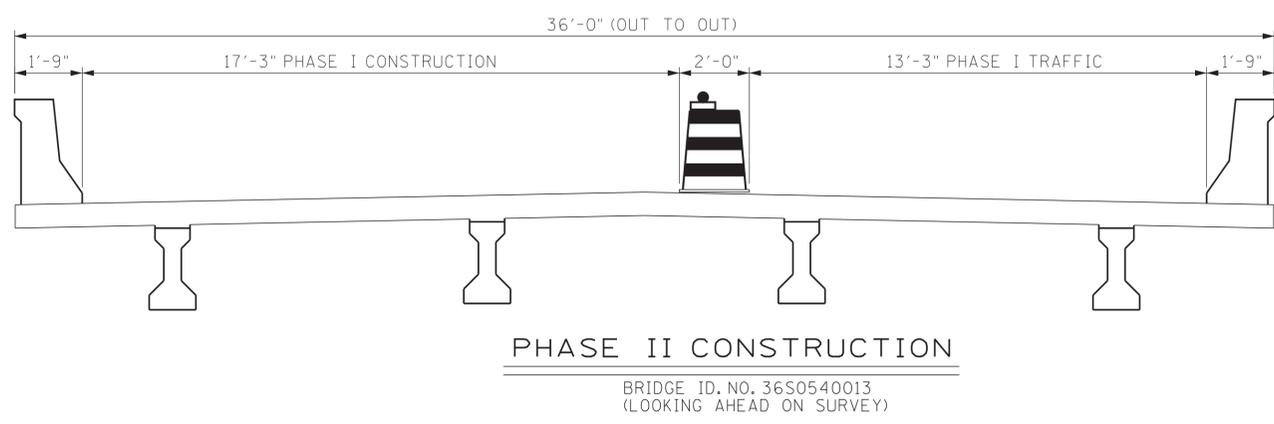
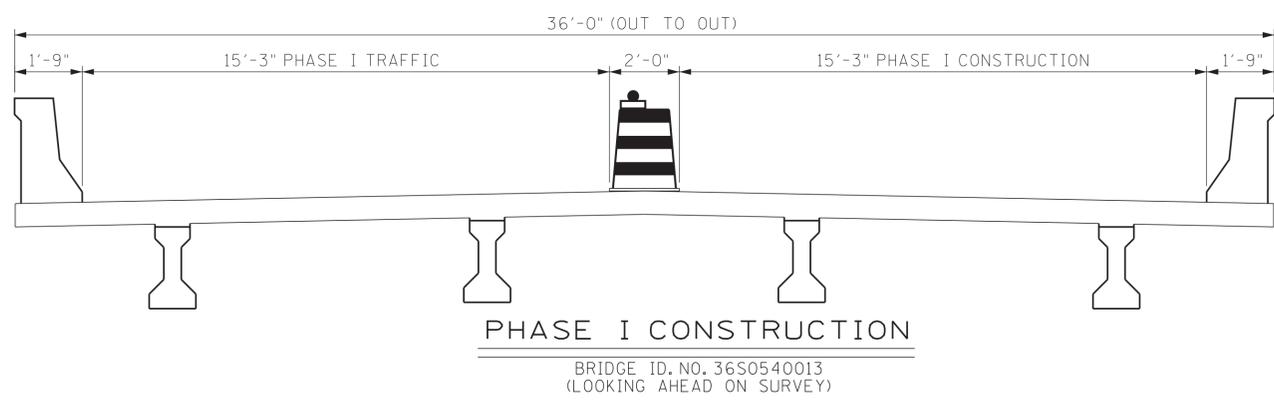
PIN NO.: 135897.00
DESIGN BY: _____ DATE: _____
DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
CHECKED BY: _____ DATE: _____



PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-7	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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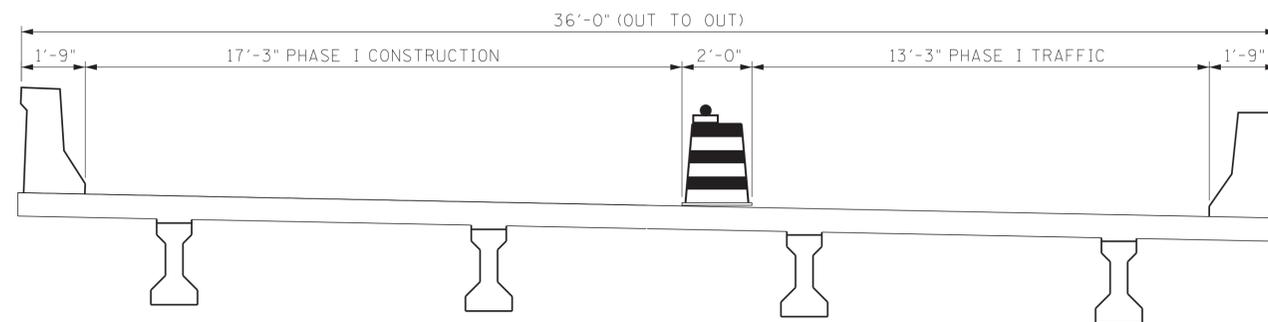
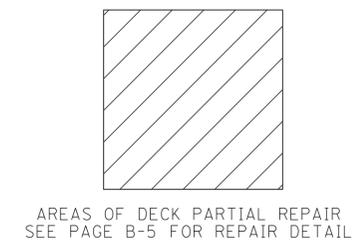
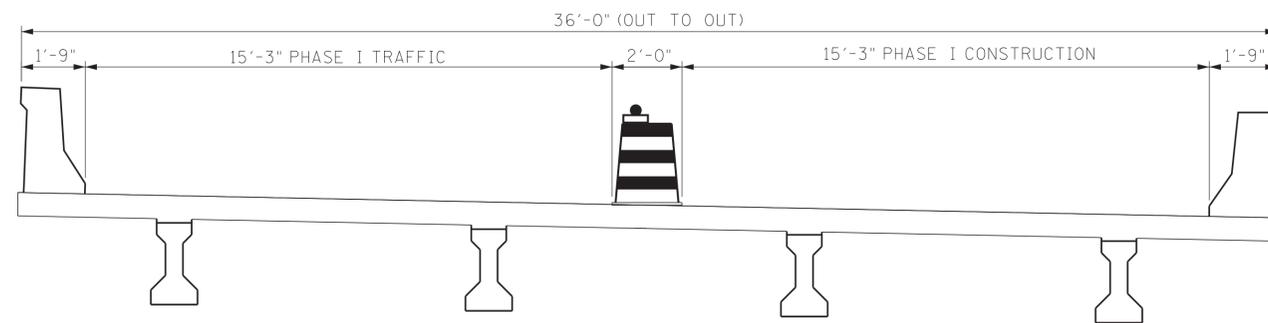
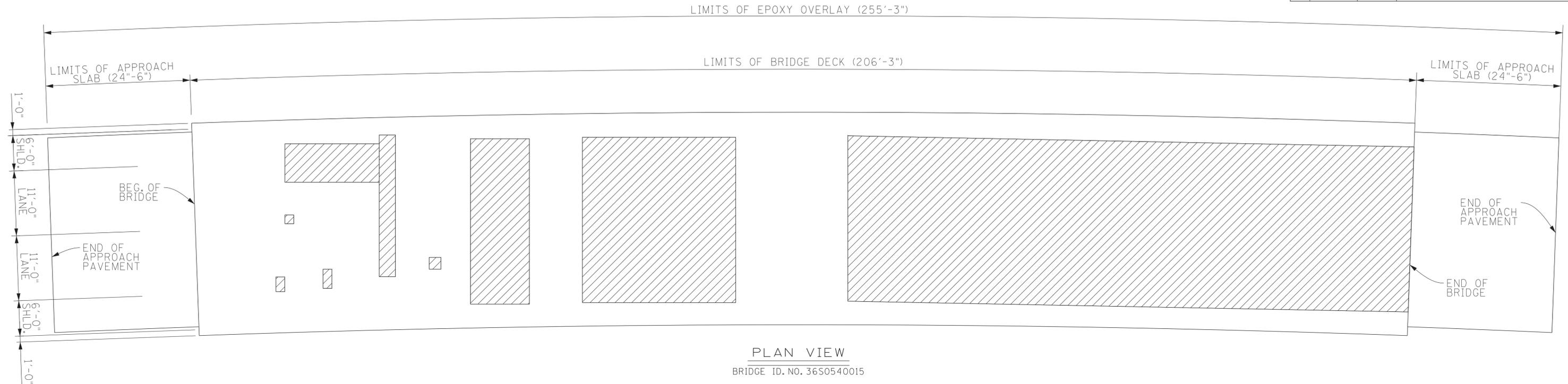
NOTE: ALL ASPHALT MUST BE REMOVED FROM ABOVE THE JOINTS.
NOTE: AN EPOXY OVERLAY WILL BE PLACED OVER REPAIRED PARTIAL AREAS.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
**PLAN VIEW AND
PHASE CONSTRUCTION**
38-SR54-22.02
OVER SANDY SLOUGH
BR. NO. 38SR0540013
HAYWOOD COUNTY
2026

PIN NO.: 135897.00
DESIGN BY: KEVIN MARTINKO DATE: 9/29/25
DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
CHECKED BY: DATE:

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-8	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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NOTE: ALL ASPHALT MUST BE REMOVED FROM ABOVE THE JOINTS.
NOTE: AN EPOXY OVERLAY WILL BE PLACED OVER REPAIRED PARTIAL AREAS.

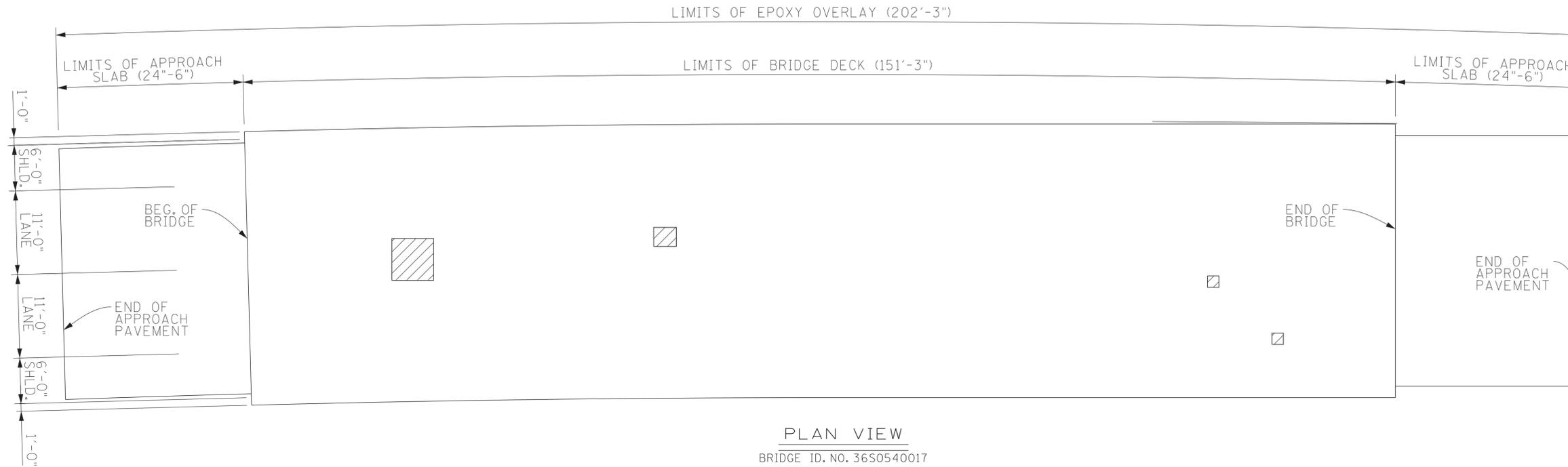
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11/04/25

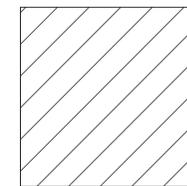
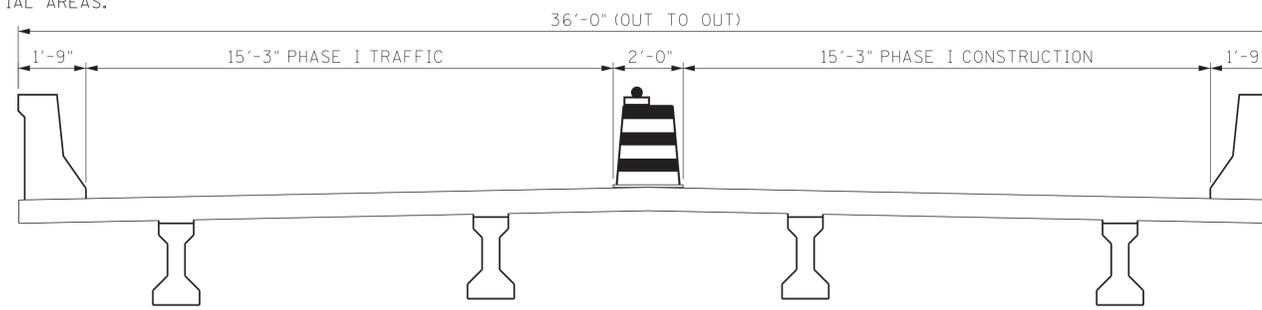
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DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
CHECKED BY: DATE:

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
**PLAN VIEW AND
PHASE CONSTRUCTION**
38-SR54-22.16
OVER OVERFLOW
BR. NO. 38SR0540015
HAYWOOD COUNTY
2026

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-9	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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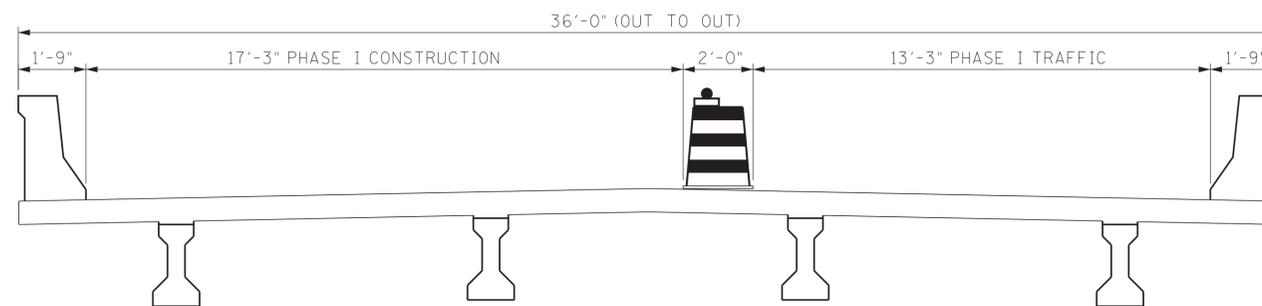


NOTE: AN EPOXY OVERLAY WILL BE PLACED OVER REPAIRED PARTIAL AREAS.

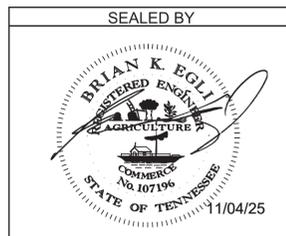


AREAS OF DECK PARTIAL REPAIR
SEE PAGE B-5 FOR REPAIR DETAIL

BRIDGE ID. NO. 36S0540017
(LOOKING AHEAD ON SURVEY)



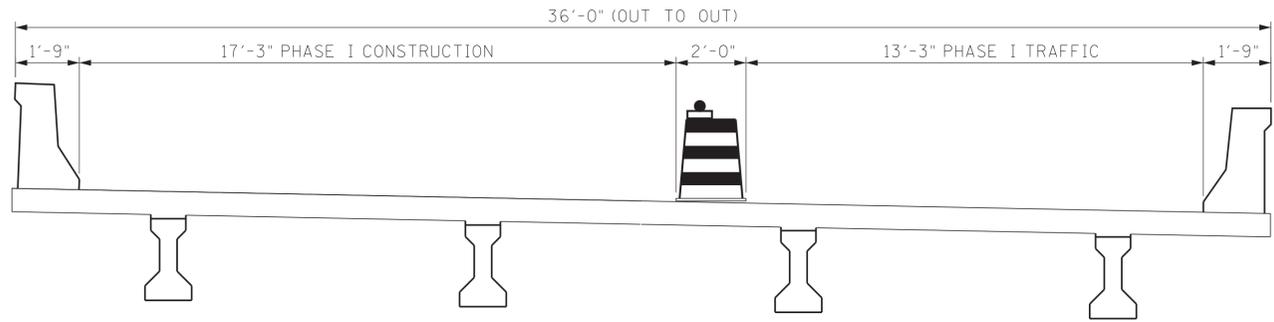
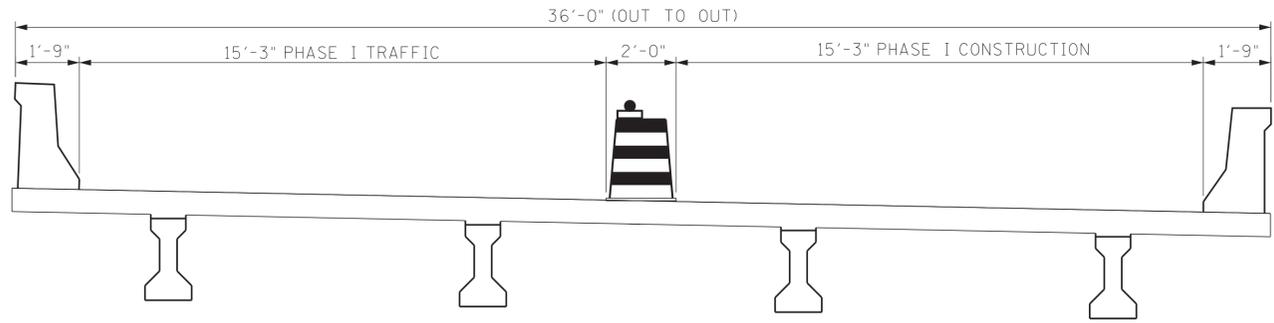
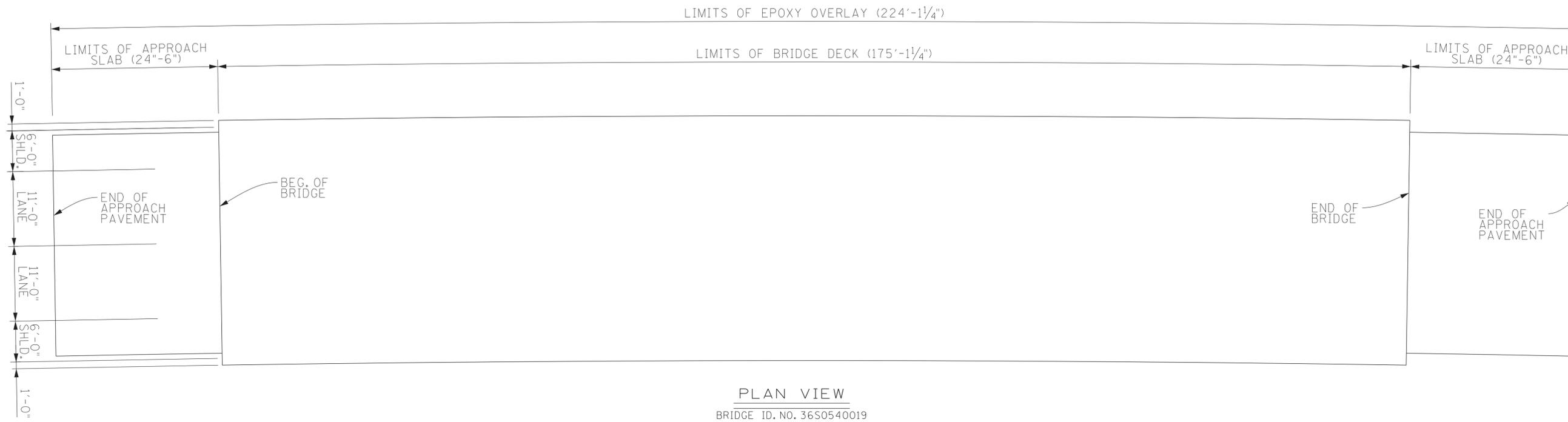
BRIDGE ID. NO. 36S0540017
(LOOKING AHEAD ON SURVEY)



PIN NO.: 135897.00
 DESIGN BY: KEVIN MARTINKO DATE: 9/29/25
 DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
 SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
 CHECKED BY: DATE:

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 PLAN VIEW AND
 PHASE CONSTRUCTION
 38-SR54-22.25
 OVER OVERFLOW
 BR. NOS. 38SR0540017
 HAYWOOD COUNTY
 2026

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-10	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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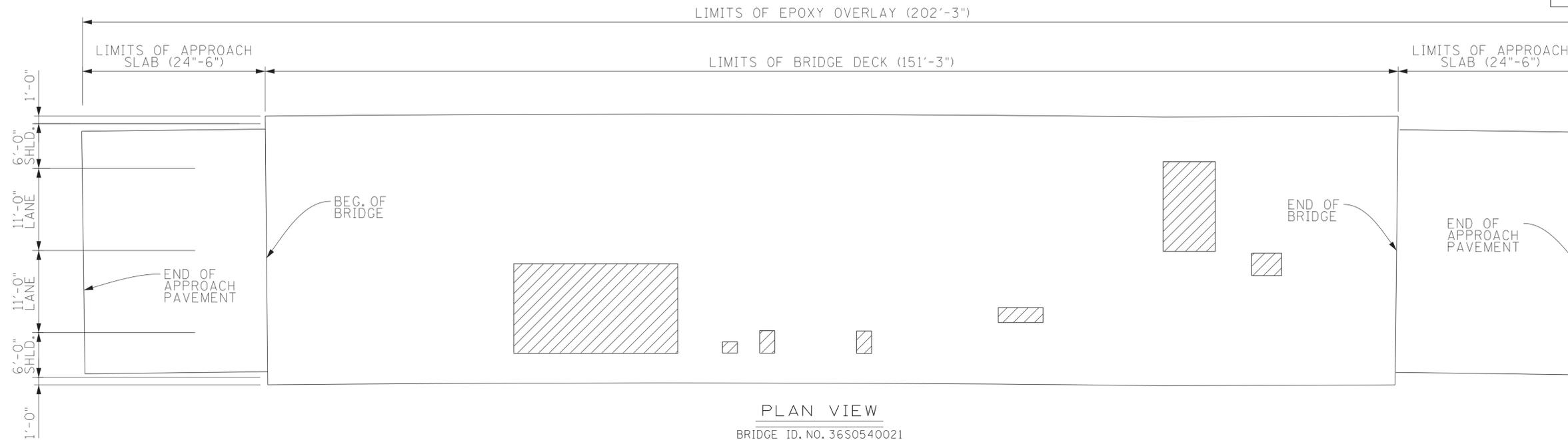
SEALED BY

11/04/25

PIN NO.: 135897.00
 DESIGN BY: _____ DATE: _____
 DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
 SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
 CHECKED BY: _____ DATE: _____

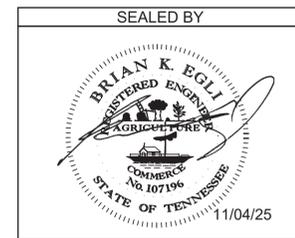
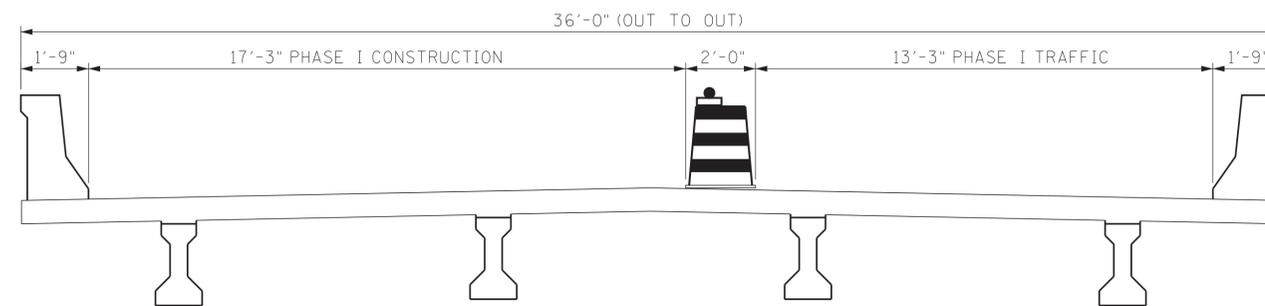
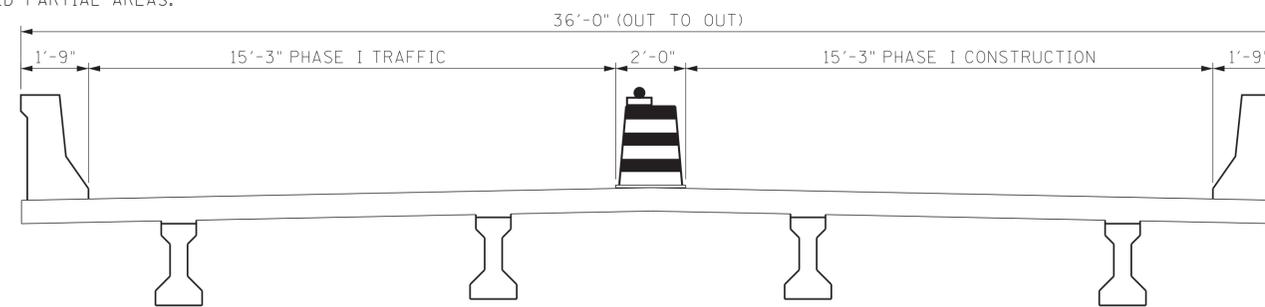
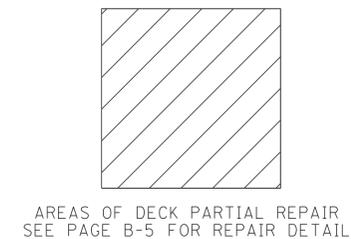
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
**PLAN VIEW AND
 PHASE CONSTRUCTION**
 38-SR54-22.36
 OVER SOUTH FORKED
 DEER RIVER
 BR. NOS. 38SR0540019
 HAYWOOD COUNTY
 2026

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-11	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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NOTE: ALL ASPHALT MUST BE REMOVED FROM ABOVE THE JOINTS.

NOTE: AN EPOXY OVERLAY WILL BE PLACED OVER REPAIRED PARTIAL AREAS.



PIN NO.: 135897.00
 DESIGN BY: _____ DATE: _____
 DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
 SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
 CHECKED BY: _____ DATE: _____

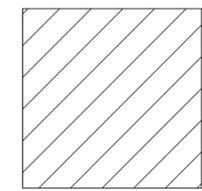
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
**PLAN VIEW AND
 PHASE CONSTRUCTION**
 38-SR54-22.49
 OVER OVERFLOW
 BR. NOS. 38SR0540021
 HAYWOOD COUNTY
 2026

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-12	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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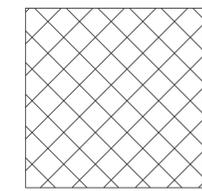


PLAN VIEW
BRIDGE ID. NO. 36S0540023

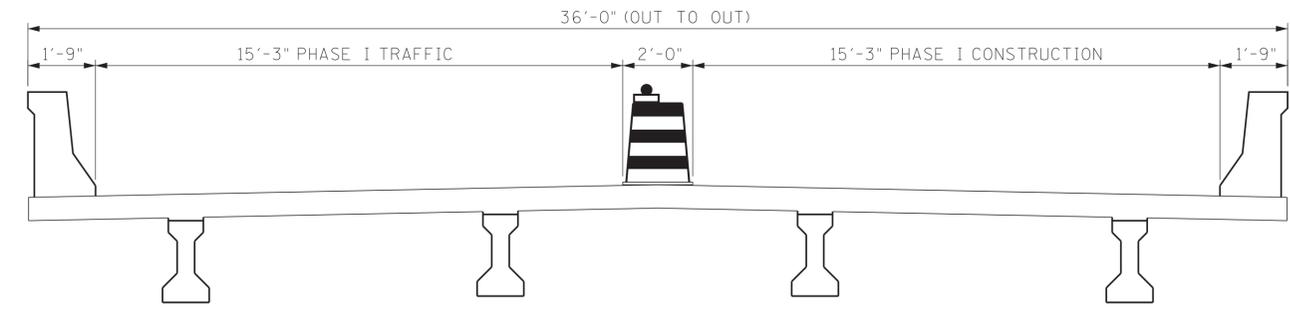
- NOTE: ALL ASPHALT MUST BE REMOVED FROM ABOVE THE JOINTS.
- NOTE: AN EPOXY OVERLAY WILL BE PLACED OVER REPAIRED PARTIAL AREAS.



AREAS OF DECK PARTIAL REPAIR
SEE PAGE B-5 FOR REPAIR DETAIL

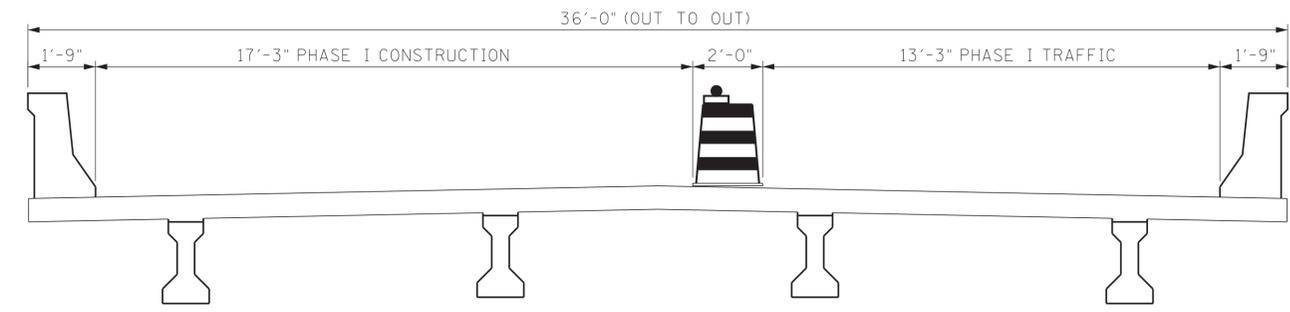


AREAS OF APPROACH PARTIAL REPAIR
SEE PAGE B-4 FOR REPAIR DETAIL



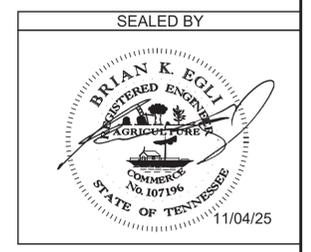
PHASE I CONSTRUCTION

BRIDGE ID. NO. 36S0540023
(LOOKING AHEAD ON SURVEY)



PHASE II CONSTRUCTION

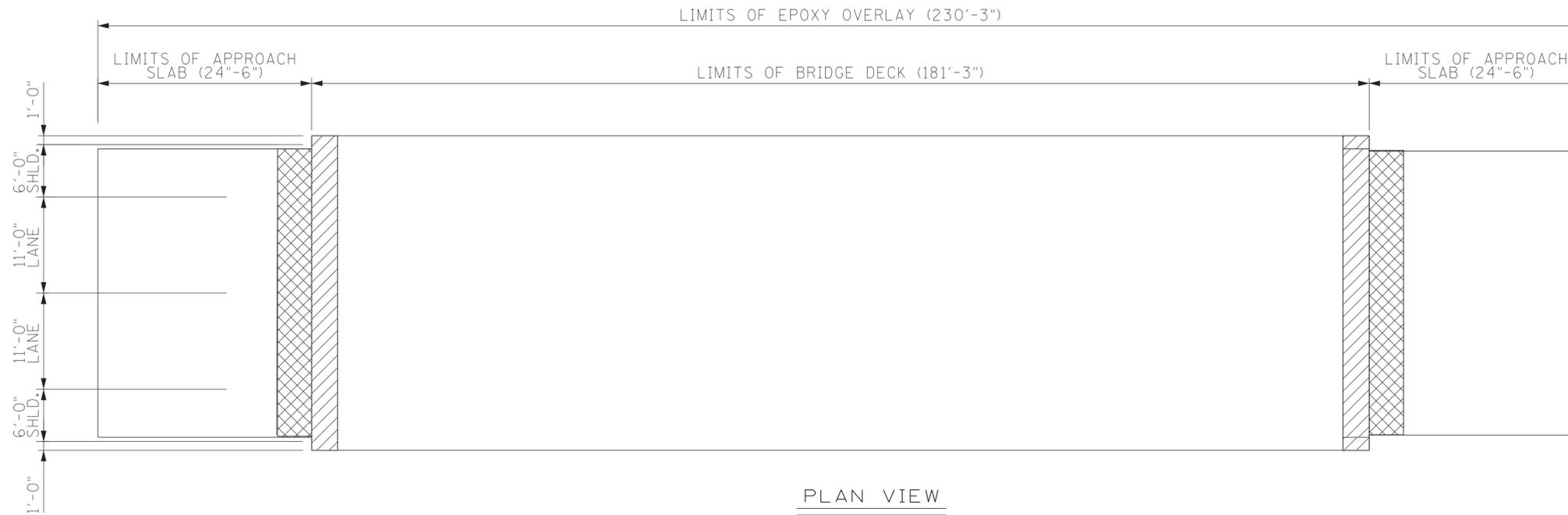
BRIDGE ID. NO. 36S0540023
(LOOKING AHEAD ON SURVEY)



PIN NO.: 135897.00
 DESIGN BY: _____ DATE: _____
 DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
 SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
 CHECKED BY: _____ DATE: _____

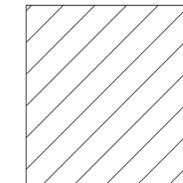
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 PLAN VIEW AND
 PHASE CONSTRUCTION
 38-SR54-22.69
 OVER OVERFLOW
 BR. NOS. 38SR0540023
 HAYWOOD COUNTY
 2026

PROJECT NO.	YEAR	SHEET NO.	
38S054-M3-005	2026	B-13	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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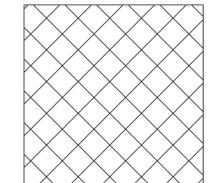


NOTE: ALL ASPHALT MUST BE REMOVED FROM ABOVE THE JOINTS.

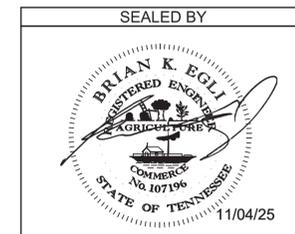
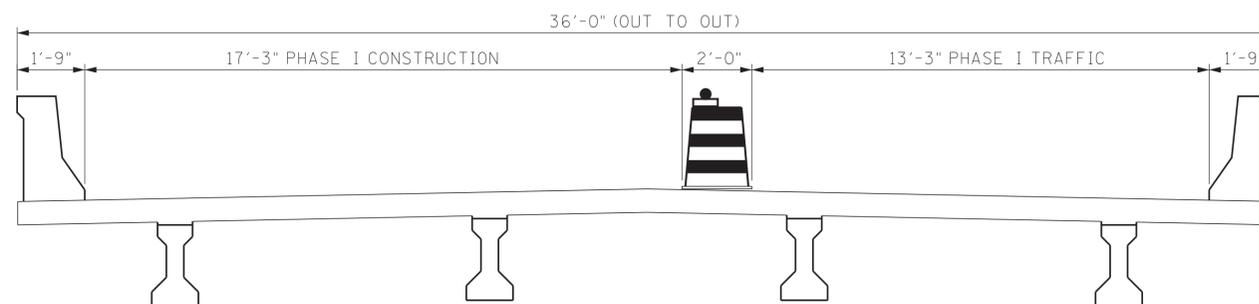
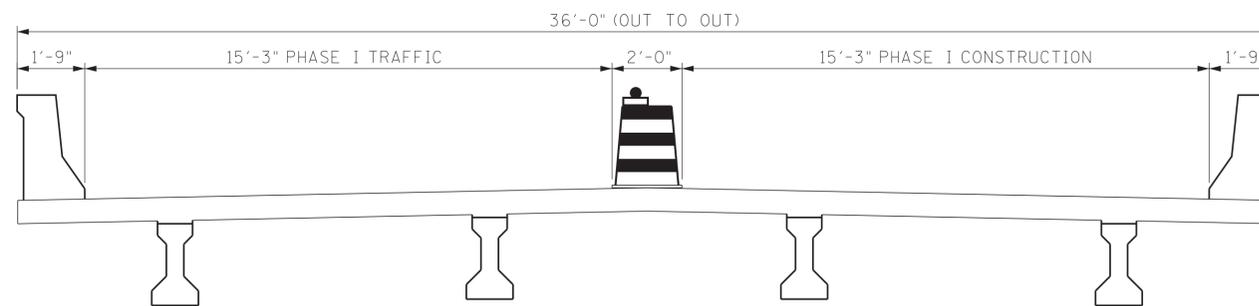
NOTE: AN EPOXY OVERLAY WILL BE PLACED OVER REPAIRED PARTIAL AREAS.



AREAS OF DECK PARTIAL REPAIR
SEE PAGE B-5 FOR REPAIR DETAIL



AREAS OF APPROACH PARTIAL REPAIR
SEE PAGE B-4 FOR REPAIR DETAIL

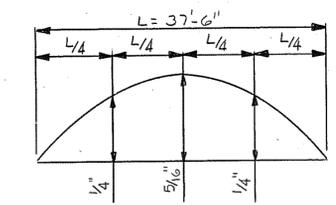


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
**PLAN VIEW AND
PHASE CONSTRUCTION**
38-SR54-22.86
OVER BRANCH
BR. NOS. 38SR0540025
HAYWOOD COUNTY
2026

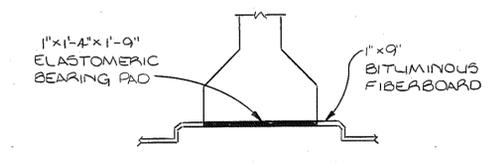
PIN NO.: 135897.00
DESIGN BY: _____ DATE: _____
DRAWN BY: KEVIN MARTINKO DATE: 9/29/25
SUPERVISED BY: BRIAN EGLI DATE: 9/29/25
CHECKED BY: _____ DATE: _____

PROJECT NO.	YEAR	SHEET NO.
BR5-243(3)	1983	

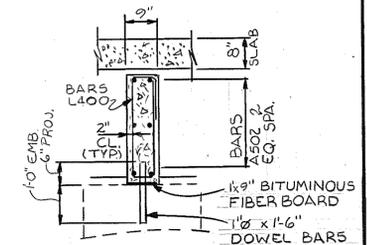
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



DEAD LOAD CORRECTION CURVE:
THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.



DETAIL A

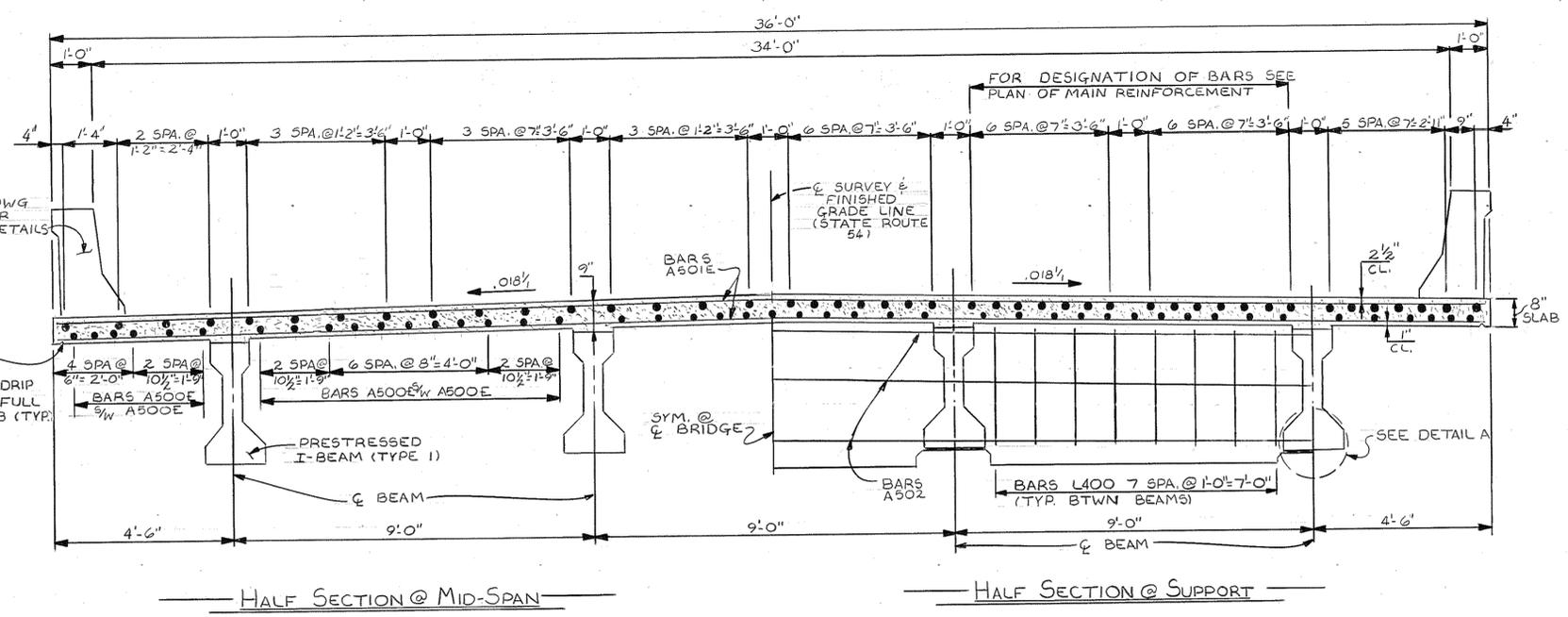


SECTION B-B

NOTE: ALL GIRDERS TO BE SUPPORTED DURING CONSTRUCTION OF SLAB TO PREVENT ROTATION.

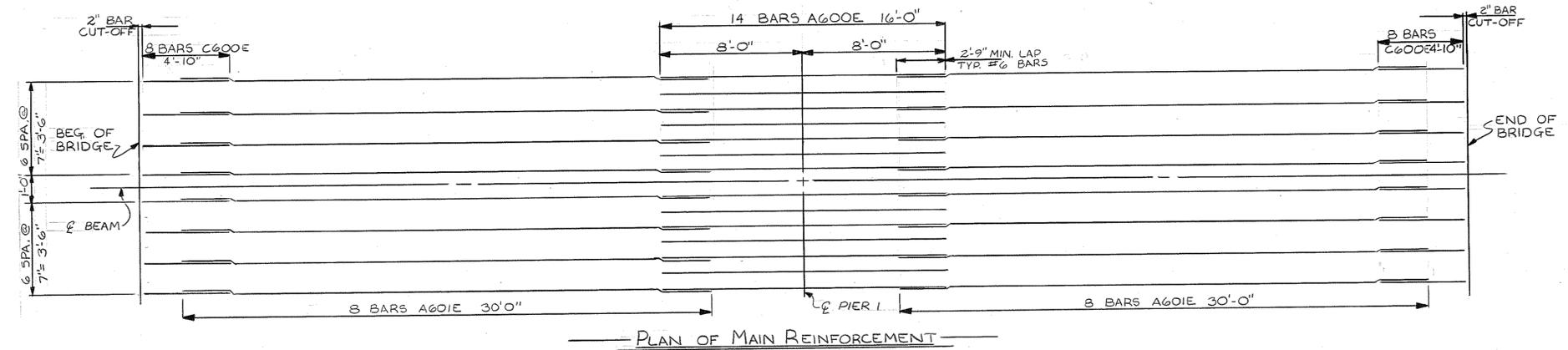
NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.

SPECIAL NOTE FOR DOWEL BARS AT PIERS: TOP OF DOWELS TO BE COVERED WITH 1/2" OF COMPRESSIBLE MATERIAL AND THE 6" PROJECTION WRAPPED WITH TWO LAYERS OF WATER PROOF PAPER.

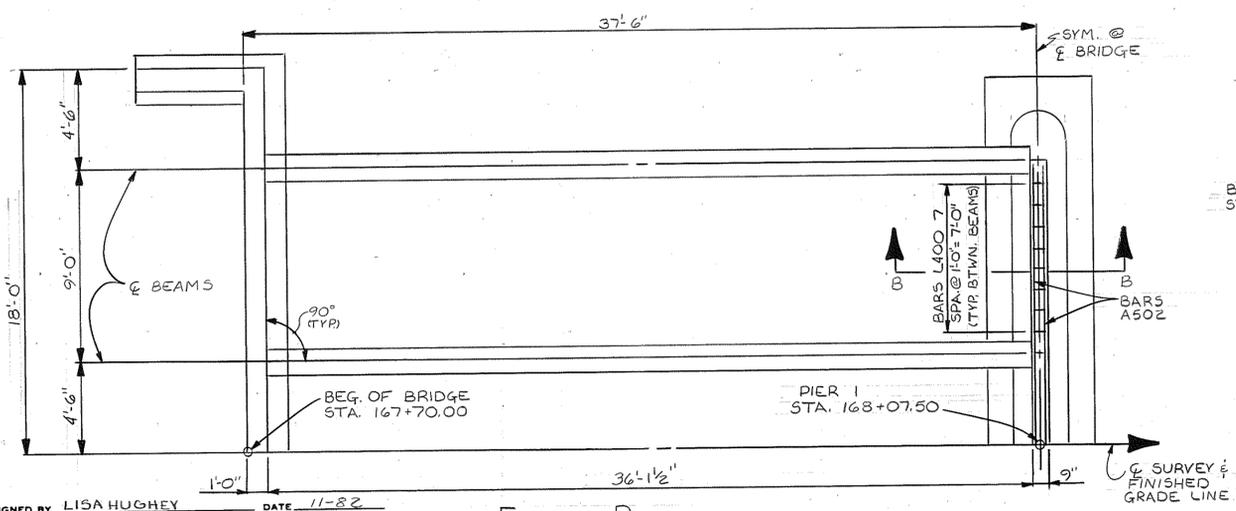


HALF SECTION @ MID-SPAN HALF SECTION @ SUPPORT

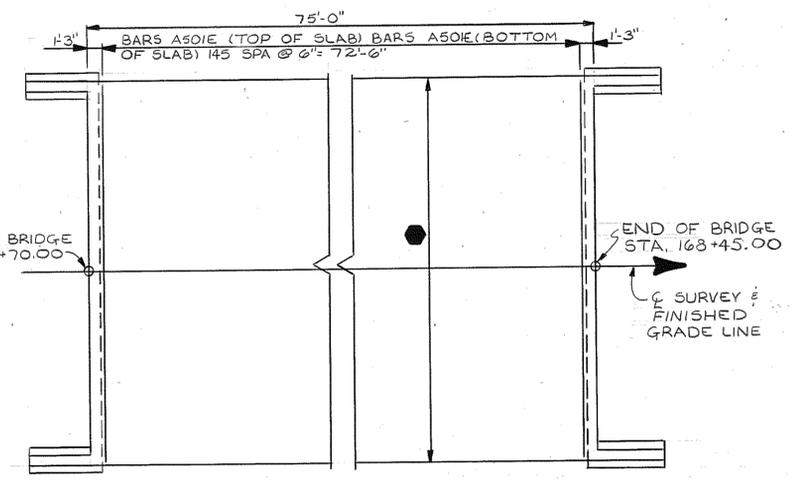
TYPICAL CROSS SECTION



PLAN OF MAIN REINFORCEMENT



FRAMING PLAN



SLAB PLAN

●: FOR BAR DESIGNATION AND SPACING SEE TYPICAL CROSS SECTION AND PLAN OF MAIN REINFORCEMENT.

ESTIMATED QUANTITIES

ITEM	EPOXY COATED REINFORCING STEEL LBS.	CLASS 'A' CONCRETE (BRIDGE DECK) C.Y.	STEEL BAR REINFORCEMENT LBS.
SUPERSTRUCTURE	19,998	83.4	6068

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
SUPERSTRUCTURE (BRIDGE NO. 1)
STATE ROUTE 54 OVER SOUTH
FORK FORK DEER RIVER (OVERFLOW)
STATION 168+07.50 (L.M. 20.95)
HAYWOOD COUNTY
1983

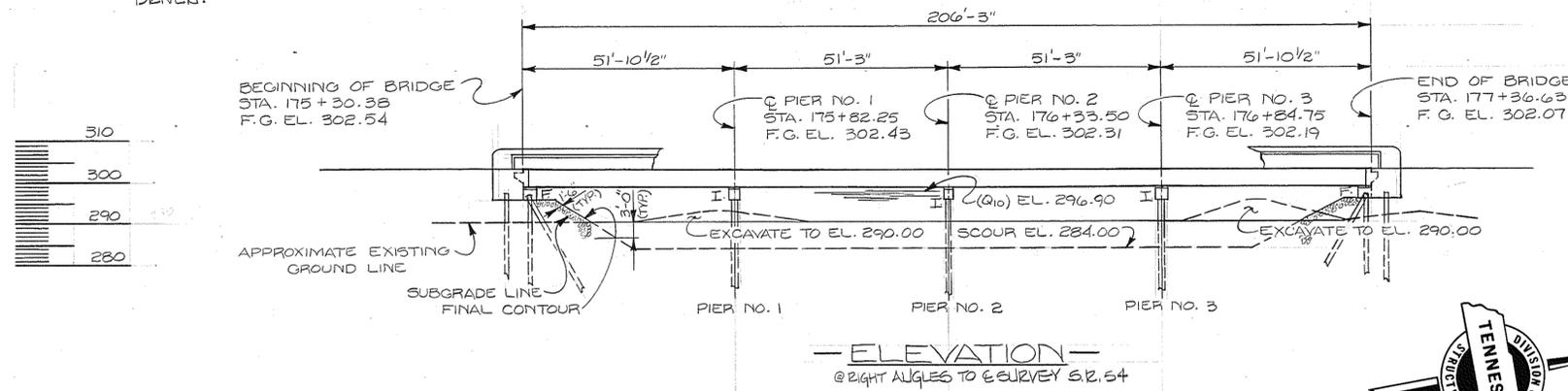
DESIGNED BY LISA HUGHEY DATE 11-82
DRAWN BY KRM DATE 5-83
SUPERVISED BY RLM MAH DATE 5-83
CHECKED BY C.N. KING DATE 6-83

CORRECT *William L. Lovell*
ENGINEER OF STRUCTURES
APPROVED *Lewis Evans*
DIRECTOR OF HIGHWAYS

PROJECT NO.	YEAR	SHEET NO.
BR5-243(3)	1983	

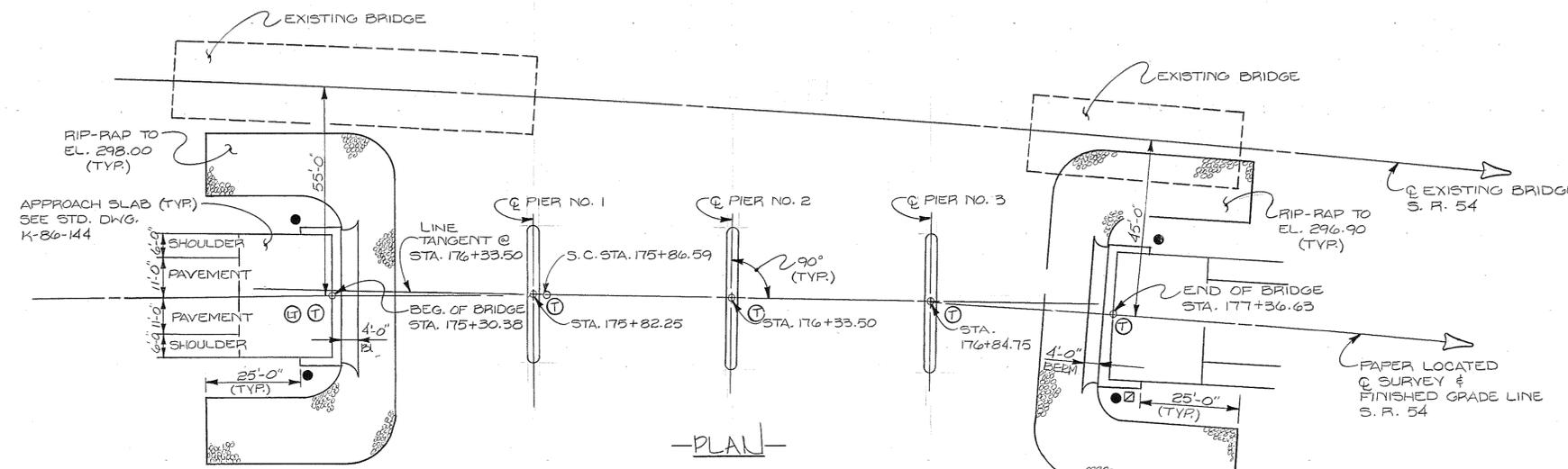
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

NOTE: THE FILLS AT THE ENDS OF THE BRIDGE SHALL BE IN PLACE AND THOROUGHLY COMPACTED BEFORE ANY ABUTMENT PILES ARE DRIVEN.



MACHINED RIP RAP SHALL BE IN ACCORDANCE WITH SECTION 709 OF THE STANDARD SPECIFICATIONS EXCEPT AS MODIFIED BY THIS NOTE. MACHINED RIP RAP SHALL BE CLEAN, CLEAR SHOT ROCK CONTAINING NO SAND, DUST, OR ORGANIC MATERIALS AND SHALL VARY IN SIZE FROM 2" TO 4". THE STONE SIZES SHALL BE DISTRIBUTED UNIFORMLY THROUGHOUT THE SIZE RANGE WITH NO MORE THAN 20% OF THE MATERIAL (BY WEIGHT) LESS THAN 4". THE THICKNESS OF THE STONE LAYER SHALL BE 1'-6" (32") AND THE SIZE GRADATION SHALL BE UNIFORMLY DISTRIBUTED THROUGH THE LAYER THICKNESS AND FROM TOP TO BOTTOM OF THE SLOPE. UPON COMPLETION OF THE PROJECT A VISUAL INSPECTION SHALL REVEAL THAT APPROXIMATELY 50% OF THE SURFACE AREA CONSISTS OF STONES 9" OR LARGER. PAYMENT WILL BE MADE UNDER ITEM 709-05.01, MACHINED RIP RAP AND QUANTITIES WILL BE BASED ON THE AVERAGE THICKNESS OF 1'-6".

LIST OF DRAWINGS	DWG. NO.	LAST REV. DATE
LAYOUT - GENERAL NOTES AND ESTIMATED QUANTITIES	M-131-10	
FOUNDATION DATA	M-131-11	
SUPERSTRUCTURE	M-131-12	
SUPERSTRUCTURE DETAILS	M-131-13	
PRESTRESSED I BEAMS	M-131-14	
ABUTMENT NO. 1 & NO. 2	M-131-15	
PIER NO'S 1, 2, & 3	M-131-16	
BILL OF STEEL	M-131-17	

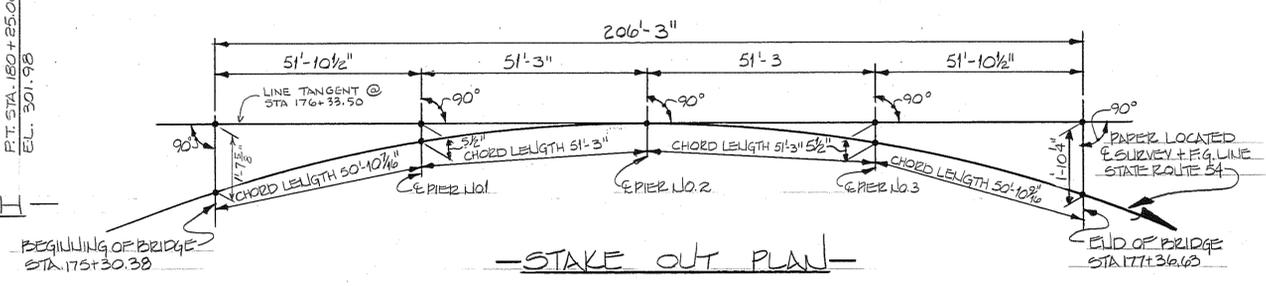
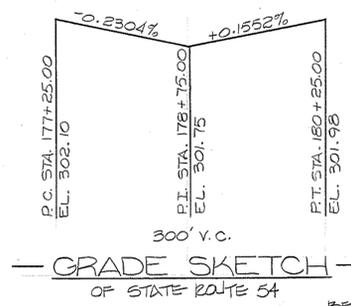


LIST OF STANDARD DRAWINGS	DWG. NO.	LAST REV. DATE
STANDARD REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS	K-80-14	8-27-76
MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS	K-85-150	1-9-75
BRIDGE RAILING ~ CONCRETE PARAPET	M-28-1	7-17-81
TENNESSEE STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	M-103-149	4-21-83
TENNESSEE STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	M-103-150	7-7-82
STANDARD DRAWING REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	K-86-144	7-17-81

RIP-RAP PLACEMENT: FINAL CONTOURS OF THE EXPOSED SURFACE OF RIP-RAP SHALL CONFORM TO THE LINES AND DETAILS AS SHOWN. THE CONTRACTOR WILL BE REQUIRED TO PLACE EMBANKMENT OR EXCAVATE TO THE PROPER 2:1 SUBGRADE SLOPE TO AVOID ANY REDUCTION IN THE DESIGN WATERWAY OPENING.

- ☐: DENOTES ROADWAY DRAIN.
- : DENOTES FUTURE GUARDRAIL (S-GR-SERIES)
- ⊕: DENOTES TEST PILE
- ⊗: DENOTES LOAD TEST

LIST SPECIAL PROVISIONS	NO.	LAST REV. DATE
105A REGARDING APPROVAL OF SHOP DRAWINGS		9-8-81
907A REGARDING EPOXY COATED REINF. STEEL		9-8-81



S.R. 54 CURVE DATA

P.C.I. = 177+02.484
Δ = 11° 21'
Dc = 2° 00'
Lc = 200'
Ts = 375.9'
Xc = 199.98'
Yc = 2.33'
LT = .133.34
ST = 66.67'
Es = 13.80'
Qs = 2° 00'
Lc = 350'
SE = 0.0425/f
Δc = 7° 00'

ESTIMATED 2002 ADT = 898
34'-0" ROADWAY WITH PARAPET

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

LAYOUT
BRIDGE NO. 2 (OVERFLOW)
STATE ROUTE 54 OVER
SOUTH FORK DEER RIVER
STATION 176+33.50
LOG MILE 21.09
HAYWOOD COUNTY

DESIGNED BY CHARLES H. HUBBARD, JR. DATE 10-82
DRAWN BY VICKY FORREST DATE 5-83
SUPERVISED BY R. L. H. M. A. H. DATE 2-83
CHECKED BY CHARLES H. HUBBARD, JR. DATE 6-83

CORRECT: *Colleen L. Laveall* 1983
ENGINEER OF STRUCTURES

APPROVED: *Lewis Adams*
DIRECTOR OF HIGHWAYS

M-131-10

CLASS 'A' GRADING 'D' = 9 C.Y.

PROJECT NO.	YEAR	SHEET NO.
BRS - 243(3)	1983	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	1-2-83	BRS	RIP-RAP REVISED

LIST OF DRAWINGS

DWG.	DWG. NO.	LATEST REV. DATE
LAYOUT OF BRIDGE	M-131-28	
GENERAL NOTES AND ESTIMATED QUANTITIES	M-131-29	
FOUNDATION DATA	M-131-30	
SUPERSTRUCTURE	M-131-31	
SUPERSTRUCTURE DETAILS	M-131-32	
PRESTRESSED I-BEAM	M-131-33	
ABUTMENT NO. 1	M-131-34	
ABUTMENT NO. 2	M-131-35	
PIERS NO. 1 & 2	M-131-36	
BILL OF STEEL	M-131-37	

LIST OF STANDARD DRAWINGS

DWG.	DWG. NO.	LATEST REV. DATE
BRIDGE RAILING-CONCRETE PARAPET	M-28-1	7-17-81
MISC. ABUTMENT AND DRAINAGE DETAILS	K-85-150	1-9-75
STD. REINFORCING BAR SUPPORT	K-80-14	8-27-76
STD. PILE DETAILS	H-5-111	11-12-82
REINFORCED CONCRETE PAVEMENT @ BRIDGE ENDS	K-86-144	7-17-81
PRECAST, PRESTRESSED BRIDGE DECK PANELS	M-103-149 & 150	4-21-83

LIST OF SPECIAL PROVISIONS

PROV. NO.	PROV. NO.	LATEST REV. DATE
EPOXY COATED REINFORCING STEEL	907A	9-8-81
APPROVAL OF SHOP DRAWINGS	105A	9-8-81
CLASS "A" CONCRETE (BRIDGE DECK)	604A	9-8-81

ESTIMATED 2002 ADT = 898
34'-0" ROADWAY WITH M-28-1 PARAPET.

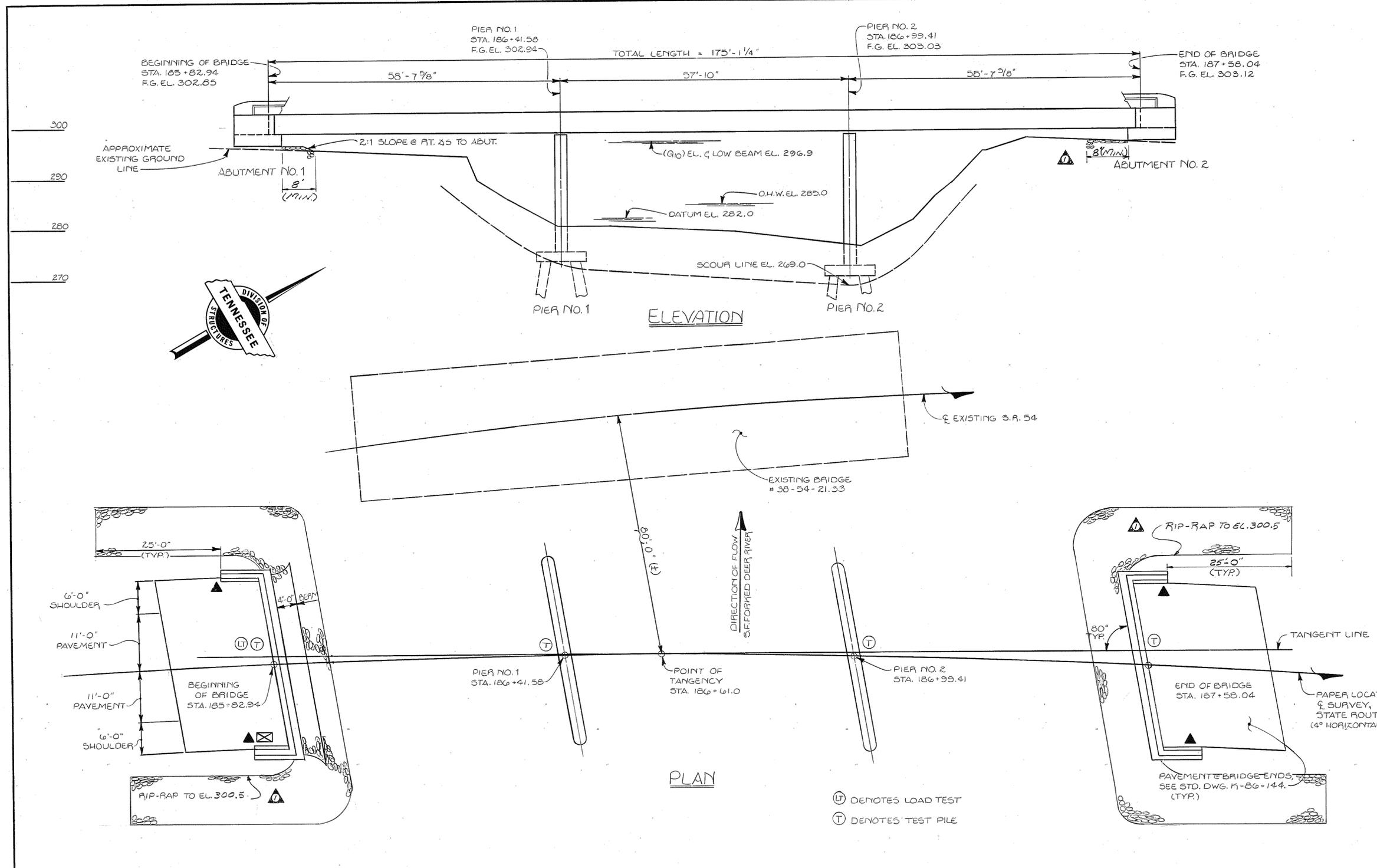
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

LAYOUT OF BRIDGE NO. 4
STATE ROUTE 54 OVER SOUTH FORK FORKED DEER RIVER (MAIN CHANNEL)
STATION 186+71.00 (L.M. 21.29)
HAYWOOD COUNTY
1983

CORRECT *Colleen L. Lowery*
ENGINEER OF STRUCTURES
APPROVED *Laurie Leland*
DIRECTOR OF HIGHWAYS

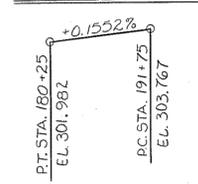
M-131-28

CLASS "A" GRADING "D" = 9 C.Y.



▲ DENOTES FUTURE GUARDRAIL ATTACHMENT
⊠ DENOTES SLAB DRAIN REQUIRED.

GRADE SKETCH



S.R. 54

S.R. 54 CURVE DATA

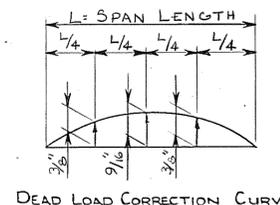
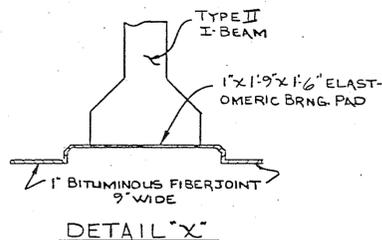
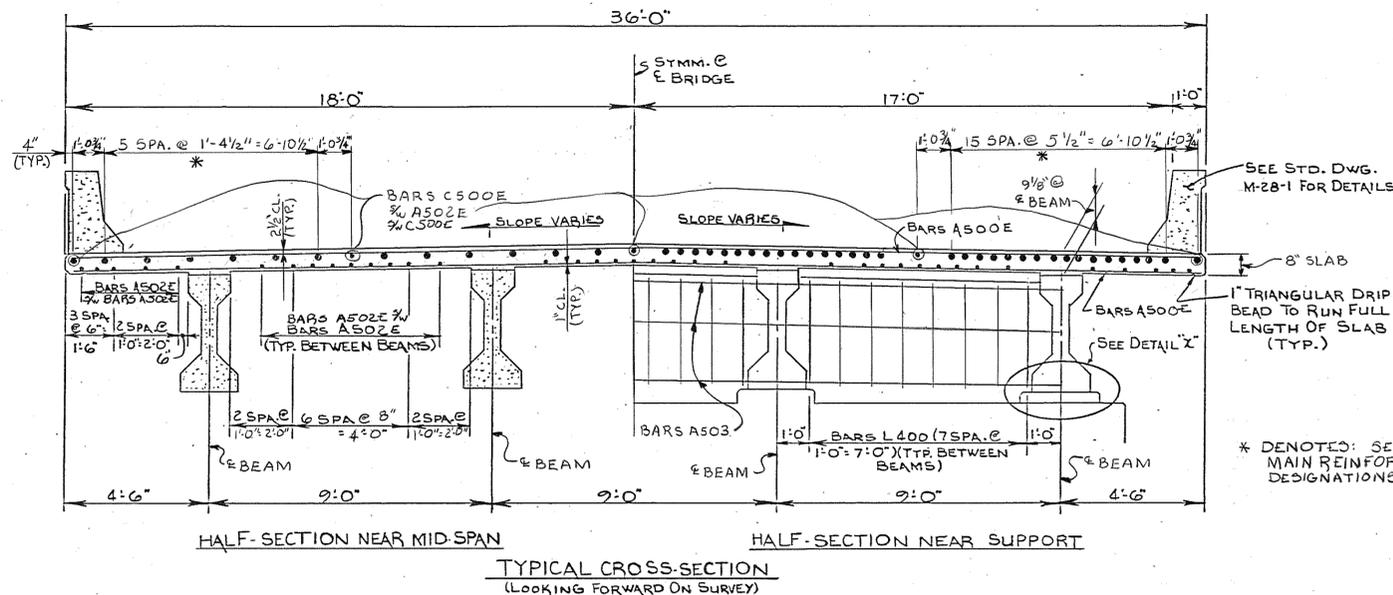
P.L.P.I. = 188+61.621	LT = 166.73'
Δ = 37° 34' RT	ST = 83.89'
Dc = 4' 00"	ES = 82.55'
LS = 250'	OS = 5' 00"
TS = 612.75'	ΔC = 27° 34' 00"
XC = 249.81'	Lc = 689.17'
YC = 7.27'	R = 1432.4'
	SE = 0.073%

HYDRAULIC DATA

TOTAL DRAINAGE AREA	1040 SQ. MI.
TOTAL DESIGN DISCHARGE (Q10)	25,800 C.F.S.
DISCHARGE THROUGH BRIDGE	9,033 C.F.S.
AREA OPENING PROVIDED	
UNDER ELEVATION 296.9	1,700 SQ. FT.
VELOCITY	3.0 F.P.S.

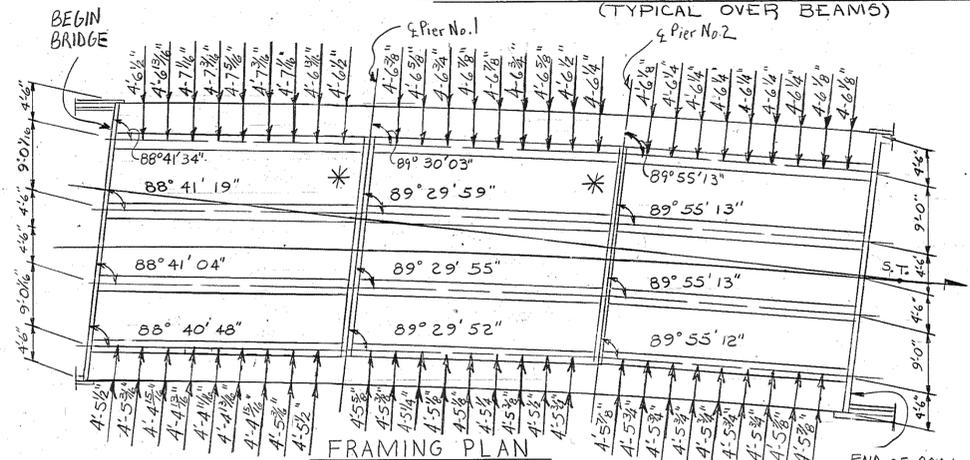
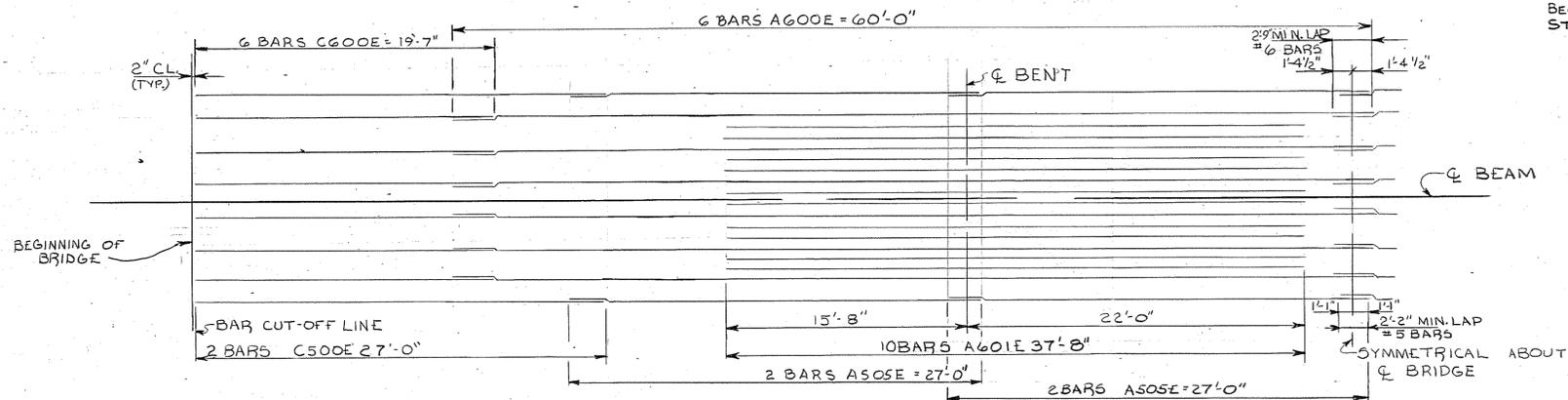
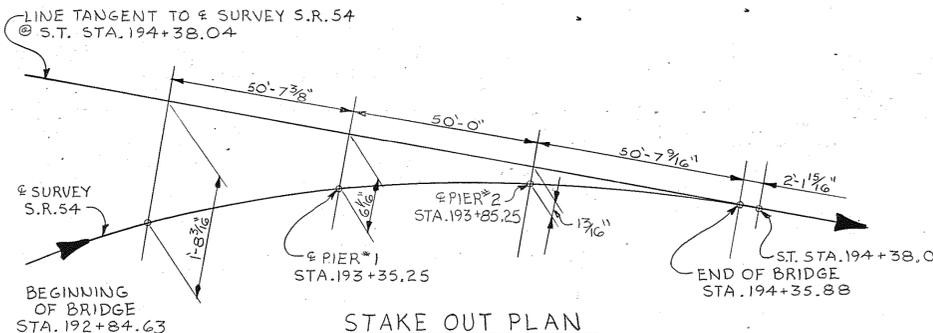
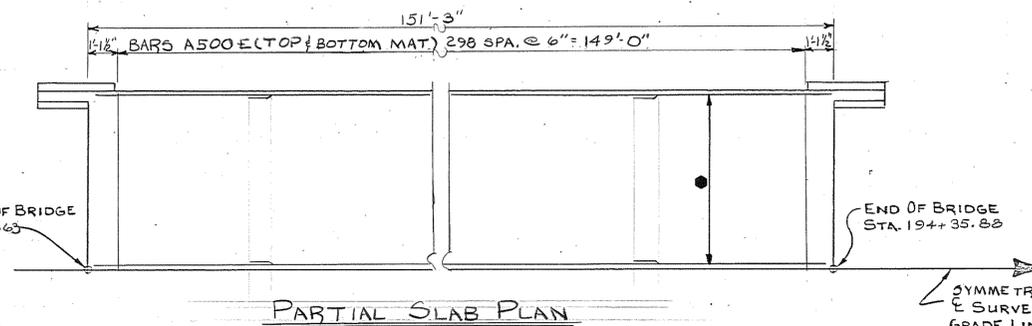
DESIGNED BY CHARLES N. KING DATE 4-83
DRAWN BY DEANNA CHAFFIN DATE 5-83
SUPERVISED BY DON HARRISON DATE 5-83
CHECKED BY CHARLES KING DATE 6-83

PROJECT NO.	YEAR	SHEET NO.	
BRS-243-(3)	1983		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



• DENOTES: FOR TOP ROW OF STEEL, SEE 'TYPICAL CROSS SECTION' AND 'PARTIAL PLAN OF MAIN REINFORCEMENT'; FOR BOTTOM ROW, SEE 'TYPICAL CROSS SECTION' THIS SHEET.

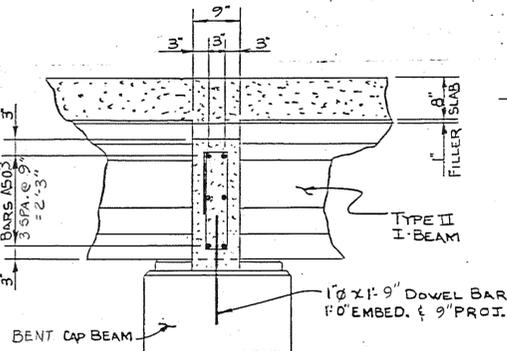
* DENOTES: SEE 'PARTIAL PLAN OF MAIN REINFORCEMENT' FOR BAR DESIGNATIONS.



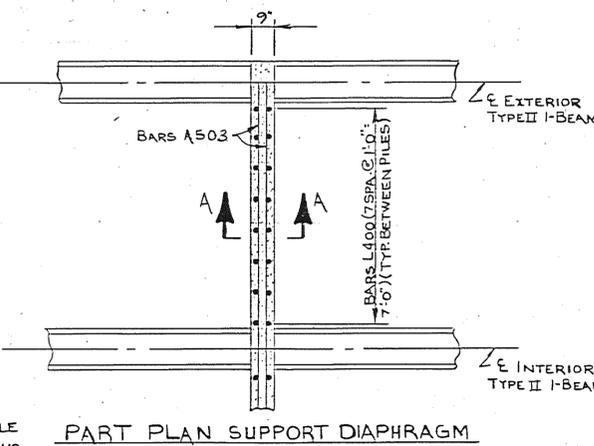
* NOTE: DISTANCE BETWEEN INTERIOR BEAMS AT PERS SAME AS THAT AT ABUT. NO. 2.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE DWG. NO. M-28-1.

NOTE: OUTSIDE EDGE OF SLAB AND BRIDGE RAIL TO CONFORM TO HORIZONTAL CURVE.



SPECIAL NOTE FOR DOWEL BARS AT PIERS: TOP OF DOWEL BARS TO BE COVERED WITH 1/2" OF COMPRESSIBLE MATERIAL AND THE 9" PROJECTION WRAPPED WITH TWO LAYERS OF WATERPROOF PAPER.



ESTIMATED QUANTITIES

	EPOXY COATED REINFORCEMENT LBS.	(BRIDGE DECK) CLASS "A" CONCRETE (C.Y.)	STEEL BAR REINFORCEMENT LBS.
SUPERSTRUCTURE	41,429	156.6	1,803

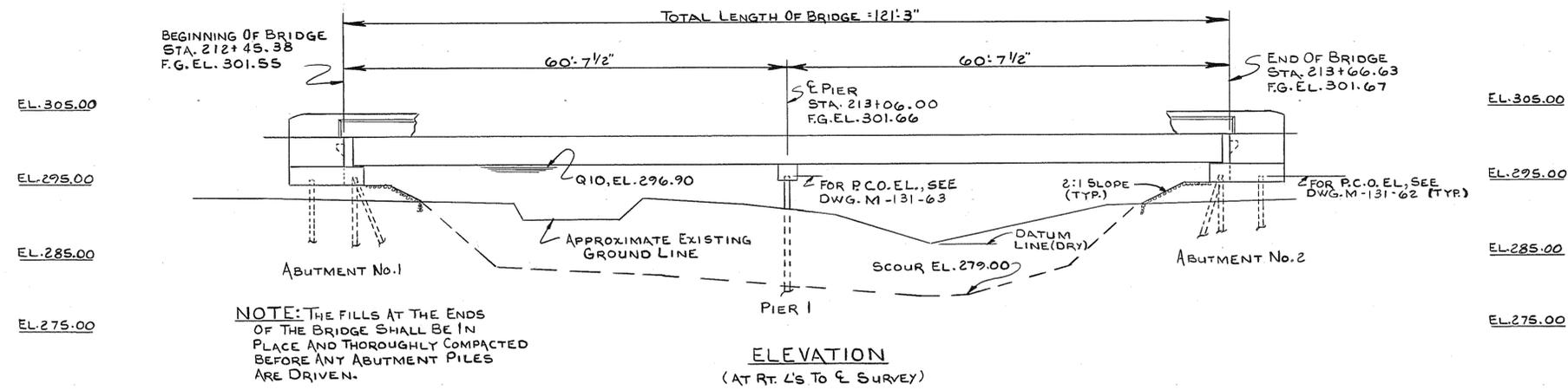
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
SUPERSTRUCTURE
BRIDGE NO. 5
STATE ROUTE 54 OVER
SOUTH FORK FORKED DEER RIVER OVERFLOW
STATION 193+60.00 (L.M. 21.35)
HAYWOOD COUNTY
1983

CORRECTED: *Clifton L. Lovell*
ENGINEER OF STRUCTURES
APPROVED: *Kevin Evans*
DIRECTOR OF HIGHWAYS

DESIGNED BY: KENT STEWART DATE: 4/83
DRAWN BY: RICK HINDMAN E.W.H.L.P. DATE: 5/83
SUPERVISED BY: R.L.H. & M.H. DATE: 5/83
CHECKED BY: KENT STEWART DATE: 6/83

PROJECT NO.	YEAR	SHEET NO.
BRS-243(3)	1983	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



LIST OF DRAWINGS

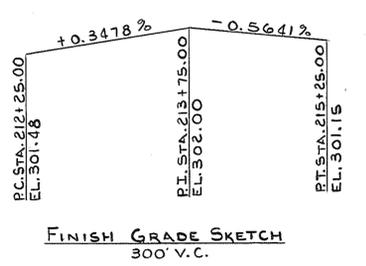
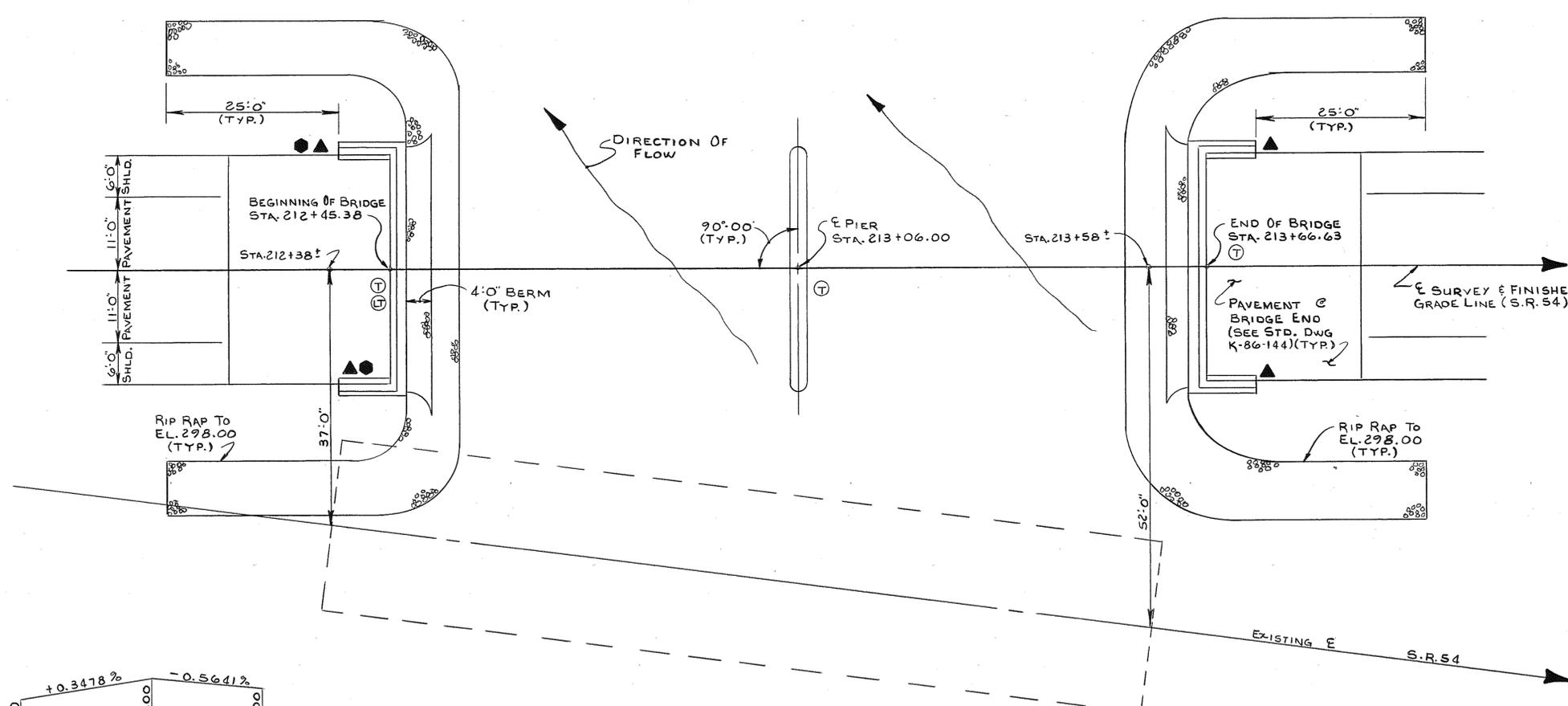
DRAWING NO.	LAST REVISION DATE
LAYOUT	M-131-50
GENERAL NOTES & ESTIMATED QUANTITIES	M-131-57
FOUNDATION DATA	M-131-58
SUPERSTRUCTURE	M-131-59
SUPERSTRUCTURE DETAILS	M-131-60
PRESTRESSED BEAM DETAILS	M-131-61
ABUTMENT NO'S 1 & 2	M-131-62
PIER DETAILS	M-131-63
BILL OF STEEL	M-131-64

LIST OF STANDARD DRAWINGS

DRAWING NO.	LAST REVISION DATE
STANDARD PILE DETAILS	H-5-111 11/12/82
STANDARD REINFORCED BAR SUPPORT	K-80-14 8/27/76
MISC. ABUTMENT & DRAINAGE DETAILS	K-85-150 1/09/75
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	K-86-144 7/17/81
BRIDGE RAILING - CONCRETE PAPER PRECAST TENNESSEE STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS	M-103-149 4/21/83 M-103-150 4/21/83

LIST OF SPECIAL PROVISIONS

DRAWING NO.	LAST REVISION DATE
REGARDING APPROVAL OF SHOP DRAWINGS	105A 9/08/81
REGARDING EPOXY COATED REINFORCING STEEL	907A 9/08/81
REGARDING CLASS "A" CONCRETE FOR BRIDGE DECK	604A 9/08/81



NOTE: ELEVATIONS SHOWN ARE BASED ON FINISHED GRADES.

- ▲ DENOTES: GUARDRAIL ATTACHMENTS
- DENOTES: ROADWAY DRAINS REQUIRED
- ⊕ DENOTES: TEST PILE REQUIRED.
- ⊙ DENOTES: LOAD TEST

HYDRAULIC DATA

AREA OPENING PROVIDED BELOW EL. 269.90 715 SQ. FT.

DISCHARGE THROUGH BRIDGE 2,180 C.F.S.

VELOCITY 3.4 FPS.

ESTIMATED ADT(2003)=898
34'-0" ROADWAY W/PARAPET

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

LAYOUT OF BRIDGE NO. 7
STATE ROUTE 54 OVER
SOUTH FORK FORKED DEER RIVER
STATION 213+06.00 (L.M. 21.81)
HAYWOOD COUNTY
1983

DESIGNED BY KENT STEWART DATE 4/83
DRAWN BY RICK HINDMAN DATE 5/83
SUPERVISED BY R.L.H. & M.H. DATE 5/83
CHECKED BY L.A. FLETCHER DATE 6/83

CORRECT *William L. Foyell*
ENGINEER OF STRUCTURES

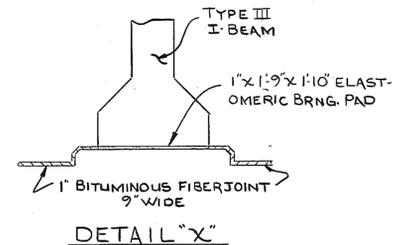
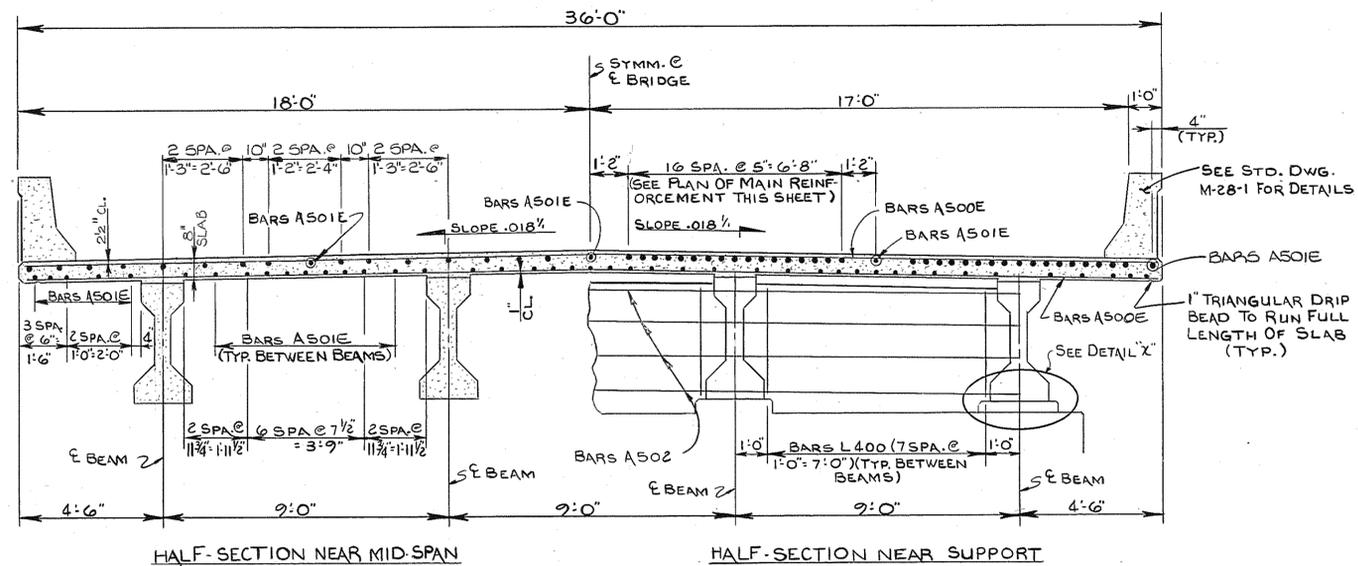
APPROVED *Paul Evans*
DIRECTOR OF HIGHWAYS

M-131-50

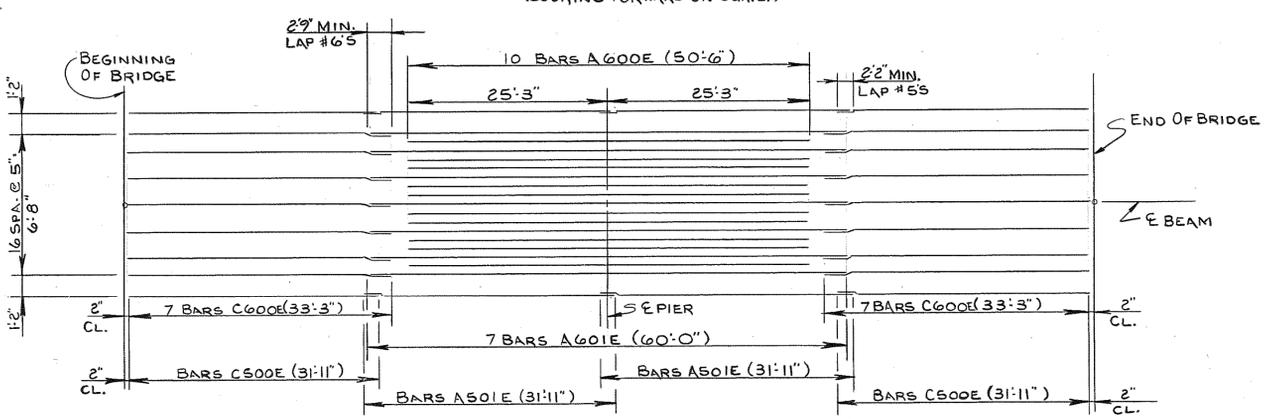
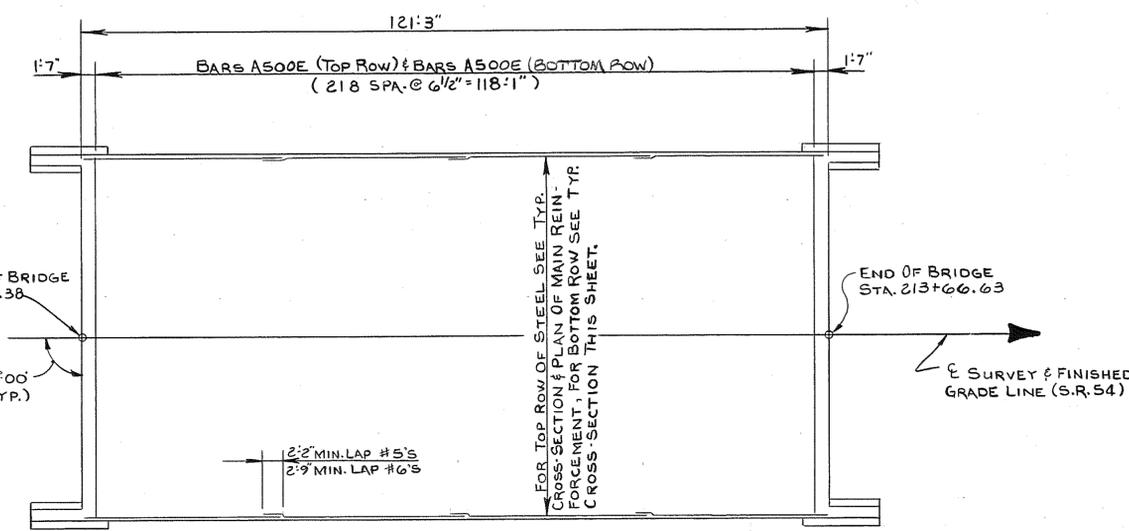
CLASS "A" GRADING "D" 9 C.Y.

PROJECT NO.	YEAR	SHEET NO.
BRS-243(3)	1983	

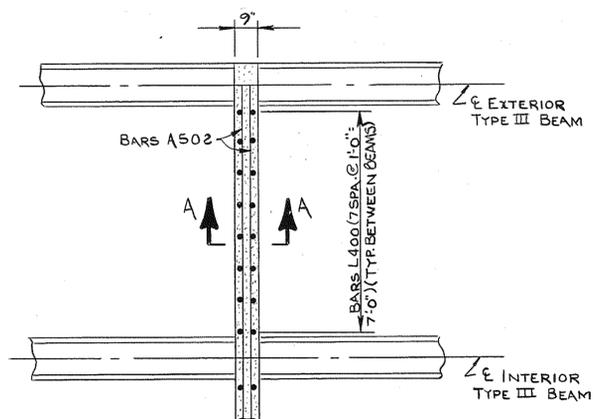
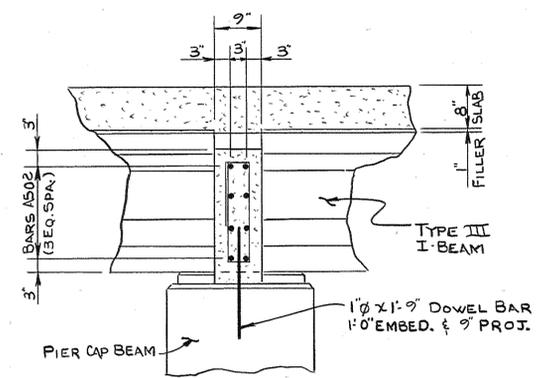
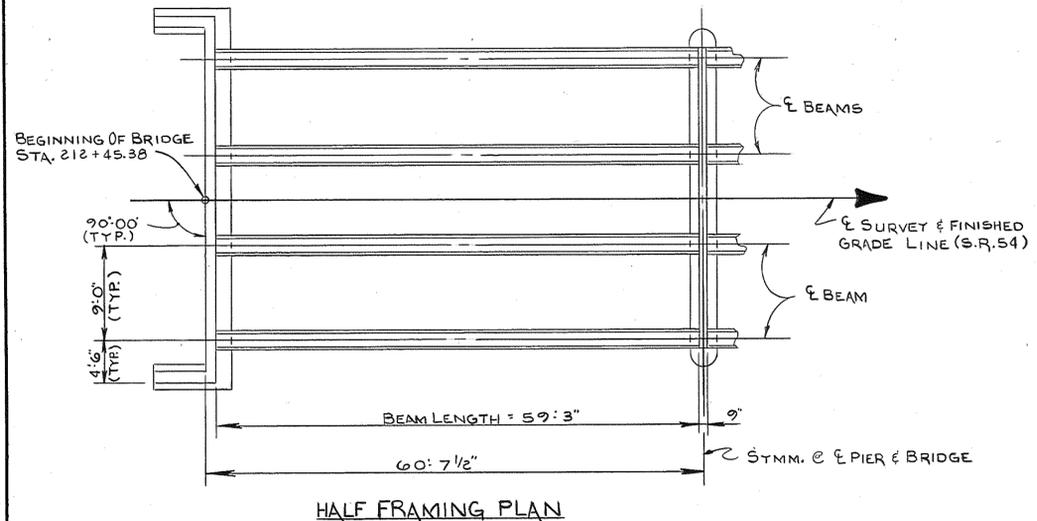
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



DEAD LOAD CORRECTION CURVE
 NOTE: THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.



NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE DWG. NO. M-28-1.
 NOTE: ALL GIRDERS TO BE SUPPORTED DURING CONSTRUCTION OF SLAB TO PREVENT ROTATION.



ESTIMATED QUANTITIES

	EPOXY COATED REINFORCEMENT LBS.	(BRIDGE DECK) CLASS 'A' CONCRETE (C.Y.)	STEEL BAR REINFORCEMENT LBS.
SUPERSTRUCTURE	31,908	138.0	1,638

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
 SUPERSTRUCTURE
 BRIDGE NO. 7
 STATE ROUTE 54 OVER
 SOUTH FORK FORKED DEER RIVER
 STATION 213+06.00 (L.M. 21.81)
 HAYWOOD COUNTY
 1983

DESIGNED BY KENT STEWART DATE 4/83
 DRAWN BY RICK HINDMAN DATE 5/83
 SUPERVISED BY R.L.H. & M.H. DATE 5/83
 CHECKED BY L.A. FLETCHER DATE 6/83

SPECIAL NOTE FOR DOWEL BARS AT PIERS: TOP OF DOWEL BARS TO BE COVERED WITH 1/2" OF COMPRESSIBLE MATERIAL AND THE 9" PROJECTION WRAPPED WITH TWO LAYERS OF WATERPROOF PAPER.

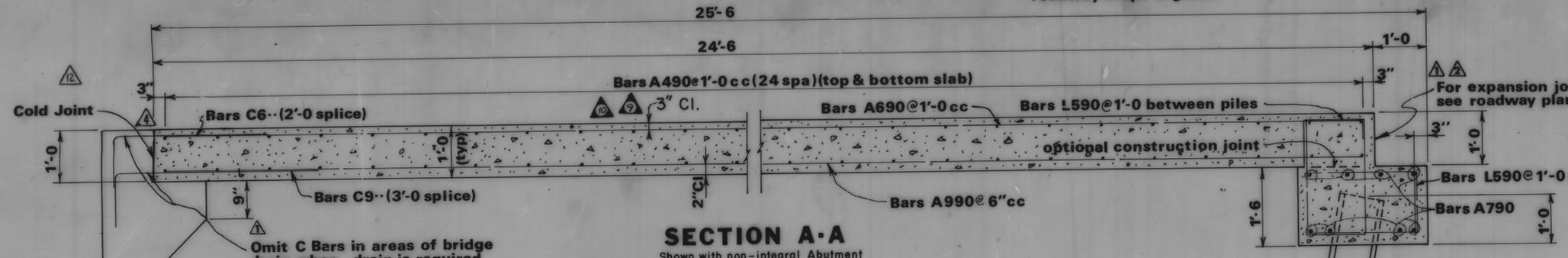
CORRECTED BY *Clifton L. Lovell*
 ENGINEER OF STRUCTURES
 APPROVED BY *Kevin Evans*
 DIRECTOR OF HIGHWAYS

95/

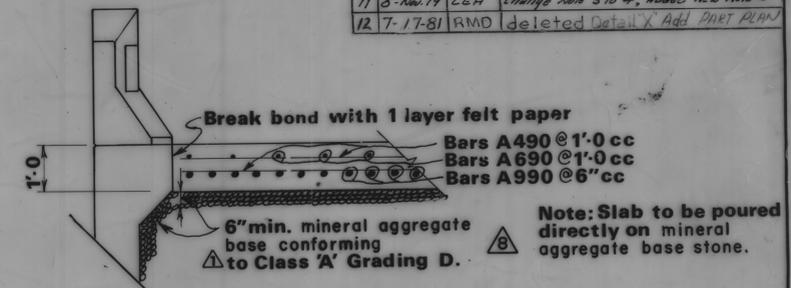
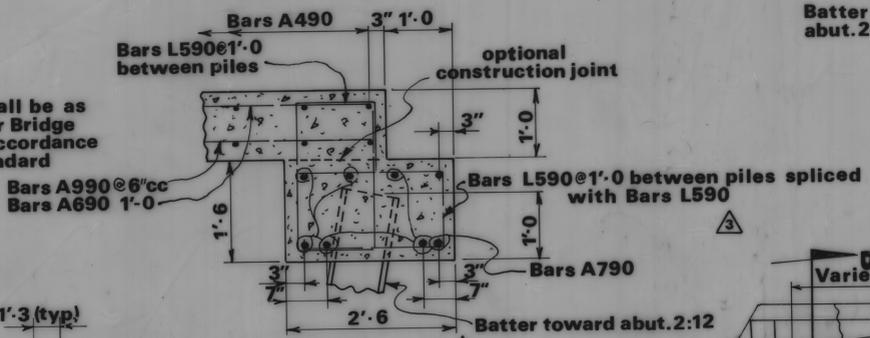
NOTE: Top of slab to conform to roadway slope & grade.

3'-8" + backwall + sine

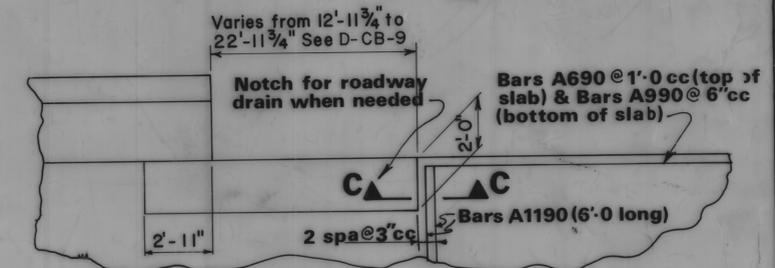
PROJECT NO.	YEAR	SHEET	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	11-16-70	CMH	Compression seal added, aggregate size changed. General notes revised & bridge drain details.
2	12-18-70	CMH	Section D-D & notes added, compression seal note removed & Expansion joint note added
3	8-12-71	RMD	Clarified spacing & no. of L590 bars
4	3-12-74	RMD	Removed V Notch
5	7-18-75	RMD	Added Detail X & Revised Note 3
6	8-18-75	RMD	Revised Roadway Drain
7	3-2-76	CPR	Changed Dwg. No. on Detail 'A'
8	6-17-76	CMH	Revised aggregate designation
9	8-30-76	RMD	Revised minimum clearances
10	5-Dec-77	EPW	Rev. Note 2 & Min. Cl. on top Re-bar
11	8-Nov-79	LEH	Change Note 3 to 4, Added New Note 3
12	7-17-81	RMD	deleted Detail X Add PART PLAN



Note: Location of notch for bridge drain shall be as shown on the Roadway Plans and/or Bridge Plans and shall be constructed in accordance with the fit-up requirements of Standard Drawing D-CB-9 or as directed by the Engineer.



"Clip corners 6" for 60° or less



Note: See State Standard Drain Details Drawing No. D CB-9

DETAIL 'A'



GENERAL NOTES

- CONCRETE: To be Class 'A' (F'c=3,000 psi)
- REINFORCING STEEL: To be ASTM A615. Bending dimensions shown are based on Grade 40. Spacing dimensions are center to center unless otherwise noted.
- SPECIFICATIONS: Standard Road & Bridge Specifications of the Tennessee Department of Highways (Current Edition)

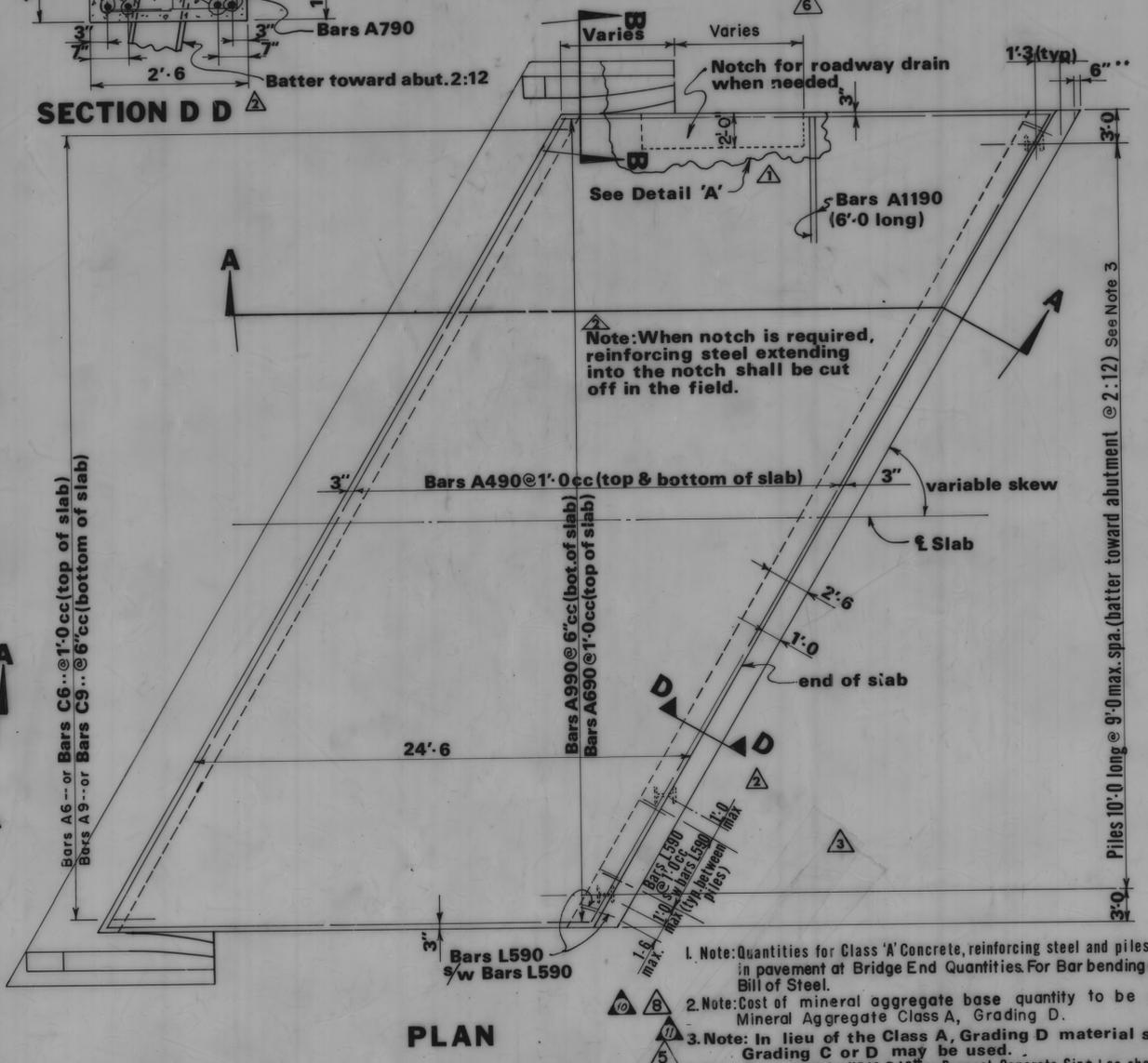
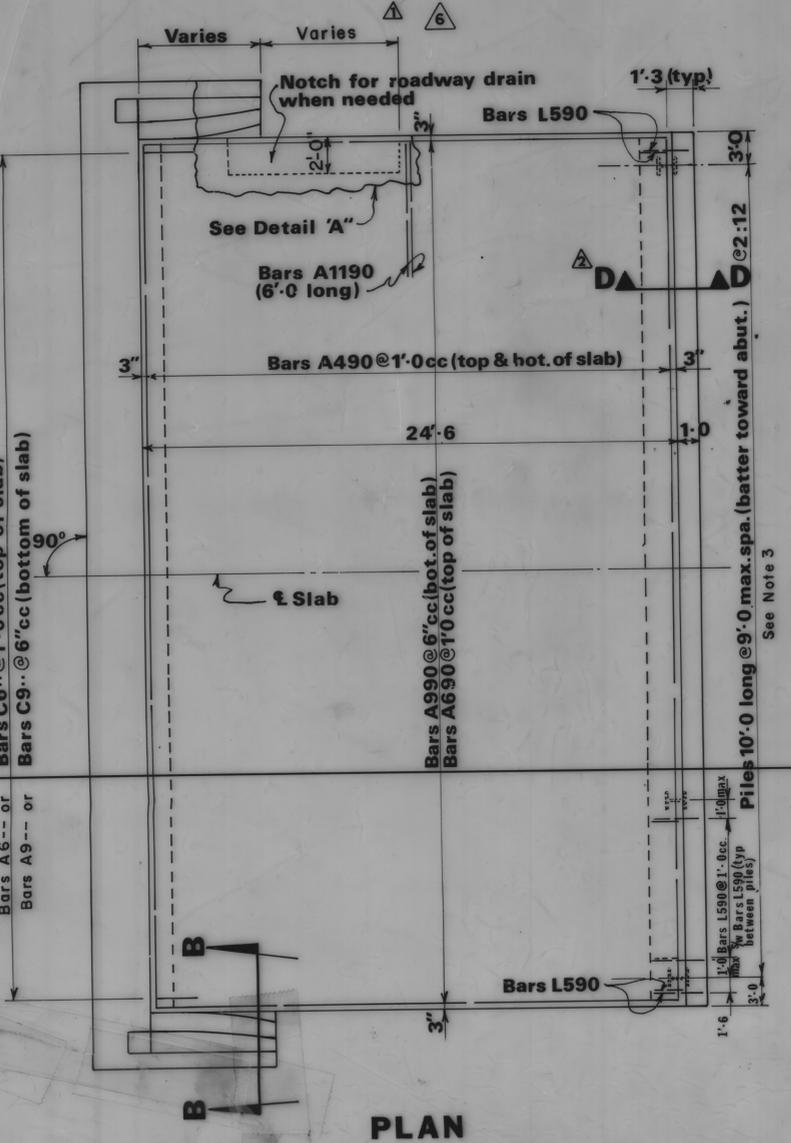
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

**STANDARD DRAWING
REINFORCED CONCRETE
PAVEMENT AT BRIDGE ENDS**

CORRECT
APPROVED
ENGINEER OF STRUCTURES
DIRECTOR OF HIGHWAYS

K-86-144

1 OF 4



- Note: Quantities for Class 'A' Concrete, reinforcing steel and piles are included in pavement at Bridge End Quantities. For Bar bending dimensions, see Bill of Steel.
- Note: Cost of mineral aggregate base quantity to be paid as Item 303-01 Mineral Aggregate Class A, Grading D.
- Note: In lieu of the Class A, Grading D material shown, Class B, Grading C or D may be used.
- Note: Piles shall be HP10 @ 42" or Precast Concrete Size I as shown in Estimated Quantities. Piles shall have a maximum length of 10'-0 regardless of bearing and shall be spaced at 9'-0 maximum. Piles shall be omitted if beam is supported on rock or rock fill or if the Abutment is integral.

DESIGNED BY C.M. Hiles
DRAWN BY G.P.M.
SUPERVISED BY C.M. Hiles
CHECKED BY D.W. Fortner

DATE 8-70
DATE
DATE
DATE 10-8-70